

THE

DISPATCH

November-December 2021

Wishing everyone Happy Holidays



COVER: An overview of Joseph A. Tringali's layout that takes him back home for Christmas any time of the year. See his story on page 18.

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NASG DISPATCH

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CHANGES

Must be reported to Dick Kramer at the addresses above and received 30 days prior to publication of the Dispatch. No replacement issues sent after that.

DEADLINES for time sensitive articles and advertising are December 15 for the March, February 15 for May, May 15 for the July issue, June 15 for September, August 15 for the November issue and October 15 for the January issue.

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President's Message

Will Holt, NASG President

THE TIME HAS COME

Admittedly promotional opportunities for S, 1:64 model railroading has been limited for at least nineteen months. There have not been train shows to attend. Many S clubs and associations have not been able to have face-to-face meetings. Even if clubs have been able to meet, there were restrictions on how many may attend, if face masks were necessary, or other limitations.

That time is ending. Clubs have resumed in-person meetings. Train shows are once again being scheduled. Among those are the National Train Show, Great Train Shows, The World's Greatest Hobby on Tour and the Greenberg Shows. Smaller local shows are starting in some areas of the country. The recent, limited resurgence of COVID-19 has slowed this, but not stopped it.

We need the members of the NASG to make every effort to attend shows and promote once again

S, 1:64 trains. Time has shown that the most effective promotion is in-person, face-to-face contact, and communication. There are many avenues for promotion

that the NASG itself participates in. If a prospective S modeler can talk to someone in S and learn via direct contact about S, we will see continuing growth.

You may say "I cannot do that." Do not sell yourself short. Think about it. Your knowledge about S is greater than someone who is not in S. You may not be able to answer every question (who can?), but you can refer the individual with whom you are conversing to other resources within S.

And the time has come that more of the S events can take place. Back in May the Spring S Spree took place. In October, the Indianapolis O Scale/S Scale meet took place. As you read this the Fall S Fest is taking place the first weekend of November.

Come 2022, the N A S G will again be able to participate in the World's Greatest Hobby on Tour Show, albeit on a shorter schedule than in the past. The shows will be in the Portland, Oregon area and the Seattle-Tacoma, Washington area on two consecutive weekends in January. The third and final show of the season will be in Indianapolis, Indiana area in mid-February.

If you live in those areas, please volunteer to help staff the booth that the NASG will sponsor at each show. Remember, it is personal, face-to-face contact that is our most effective tool for promoting S.

Do not sit on the sidelines. Get active.



New Orders

Richard Caugherty, Editor

HOW CAN ANOTHER YEAR HAVE GONE BY?

My wife and I were just looking over the calendar for the next two months and wondered where the time had gone. It seems like yesterday that we had just hung a 2021 calendar. And it is not only years that fly by, but weeks and months.

Funny when we were kids how slow the day passed between Thanksgiving and Christmas. Now one is barely over than the next one is here. We just got a gift suggestion list from our grandchildren in Houston, and I have no idea what my grandson's items are.

We used to be so up on things for our own children, but not so interested in toys, games and electronics now. Also, the advertisements are no longer geared to our age group.

I suppose the gap came after our children grew up, got married and had children. We were more interested in our jobs and looking forward to retiring. When it was time to buy things for grandchildren, we discovered

that a learning void existed.

Now we get lists from Amazon, and it does make it easier to shop on the computer. I still miss going to stores and hearing holiday music. Do not miss the hunting for a parking place or the crowds. Now, we have shopping finished before Thanksgiving. My shopper even starts in the summer.

Since I cannot have those old experiences, nor do I really want them; I will just put on some music and settle back in a comfortable chair and watch some trains run around my Christmas layout.

All my best wishes to you for the coming holidays. May you enjoy them with family and friends. May you stay safe and healthy in the coming year.

THANK YOU TO OUR CONTRIBUTORS

I want to say thank you to all those of you who have authored articles for *The Dispatch* this year and in the past. Some of you are long-time contributors and some are new. Without your articles and photos there would be no magazine.

That said, I am still looking for articles and photos about your layouts. We have many how to articles, but not enough about your miniature empires.

PRODUCTS FOR S GAUGE

Jeff Madden, New Products Editor

Precision Vintage Classics, www.pvc-sn3.com - lists several Sn3 freight cars including new ones, a D&RGW gondola also



a WP&Y open container. Plus they list some



resin cast items including an SP oil tank at Laws for \$70; an SP oil spout for \$35.

P V C ,
9 9 3 8

257th St. CT E, Graham, WA 98338 Phone: 253-

875-1684.

Model Tech Studios: www.modeltechstudios.com -

They list lots of small details and figures that come RTR painted. For



example, a built up baggage cart is \$36.99. Two station agent figures list for \$19.99. Many other RTR and painted figures list for \$10.99 each. Interior store shelving runs \$27.99. Four chairs are \$10.99. MTS LLC, P.O. Box 1497, N, Hamp-



ton, NH 03862

Leadville Designs:

www.leadvilledesigns.com - Lists a 1:64 Colorado 34 foot 1892 St. Charles 34' trussrod wood boxcar kit for \$79.95 each. These are laser cut wood with etched brass parts.

Lionel's Catalog II is now available to view online

go to <http://catalogs.lionel.com/2021/C2/> The S-gauge pages begin on page 81. The Catalog II offerings, all for delivery in 2022, are T&P GP7 FlyerChief with Bluetooth 6-pc Freight set; Clinchfield GP7 FlyerChief with Bluetooth 6-pc Coal Train Freight set;



4 additional GP7s: BN, Chessie, LV, and Wabash; five 2-Bay Covered Hoppers: Andersons, B&O, Reading, Chessie, and SP; five Boxcars: CofGA, Conrail, N&W, NS, and WP("Feather"); four 3-Dome Tank Cars:



PC; Frac-King Oil; SHPX; and US ARMY; three Log Dump Cars: Alaska RR; BN; and UP; "Land of the Free" Musical Boxcar; "Hobo and Bull" Action Boxcar Pair (NYNH&H and CNJ boxcars); T-Rex Oil Assortment: Unloading Flat Car with fuel



tank truck, Single Dome Tank Car, Elevated Oil Storage Tank and Ground Level Oil Storage Tank and three Cabooses: ATSF, RG, and US ARMY. Lionel LLC, Attn. Customer Service, 620 Performance Drive, Concord, NC 28007 Phone: 586-949-4100



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NASG 2021 ANNUAL AWARDS

Will Holt, President and the NASG Board of Trustees are pleased to announce the winners of the 2021 NASG awards.

Bernie Thomas Memorial Award - Jamie Bothwell;

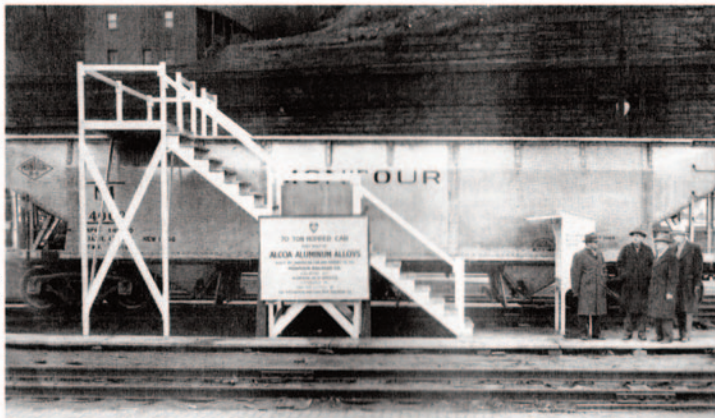
Charlie Sandersfeld Manufacturing Award - RiverRaisin Models

Perles Publication Award - Jerry Holmes and Cody Grivno
for *Worth the Wait-the C&O Allegheny Subdivision in 1952*

Josh Seltzer Electronic Media Award - Andrew Malette

Trustee's Award - Joseph Kimber

Photo courtesy of Gene P. Schaeffer



**IT'S TIME TO
PRE-ORDER YOUR
2022 S Spree
Event Car**

The Pittsburgh S-Gaugers will host the **2022 S Spring Spree** at the Washington County Fairgrounds in Washington, Pennsylvania, just 30 minutes southwest of downtown Pittsburgh.

May 13 and 14, 2022

Next year's Spree Car celebrates two great Pittsburgh companies:

The Montour Railroad and The Aluminum Company of America.

The car will be Montour Railroad #14000, a unique aluminum triple hopper built by ACF for ALCOA in 1946. This car had a productive 30-year career on the Montour and was eventually bought back by ALCOA.

Price per car is \$52.00 plus \$10.00 shipping (no shipping charge if the car is picked up at the Spree)

American Models will produce this car - available with hi-rail wheels and couplers or NASG RP25 wheels.

Order deadline is February 1, 2022 Order form is available at www.SSpree.info

Building a Soo Line



Wooden Caboose

PHOTO 1: Here's my completed carbody, ready to be reinstalled onto the No. 930 frame.

Jack Halbleib, Chippewa Falls, WI
photos by the author

After modifying an American Flyer 283 Pacific Shell to represent Soo Line Pacific 2719, I decided I needed to find an appropriate caboose to go with it. I wanted a wood-sided caboose, so I looked online and found a beautiful shorty wood-side HO Model by Centralia Car Shops reviewed in the May 2002 issue of *Model Railroader*. I used the images in the review to guide my conversion, which I will outline in the following paragraphs.

I began with a standard American Flyer No. 930 caboose. I pulled out the four pins securing the car shell to the floor. Then, being me, I did things the hard way, destroying one shell by removing the entire roof before the better/easier solution occurred to me, as follows.

Because the cupola is offset rather than centered, the solution is to cut a piece out of the roof that includes the cupola and one portion of the roof as a single piece. Then reverse it and reattach it to the shell.

I marked the location of the roof cut

with a 1"-wide piece of masking tape placed it about 1/16" ahead of the cupola base. Using the tape edge as a guide, I used my razor saw to cut along the tape I made another cut on the opposite end of the cupola just 1/16" inch from the cupola base.

Using my hobby knife, I carefully and repeatedly scribed along the top of the side ridge on each side allowing me to lift out the piece with cupola and turn it around. This gave me the correct position for my new offset cupola.

Based on the review photos and my caboose body's measurements, I made a template out of a piece of styrene to help me keep the window size consistent. Using this template as a guide, I cut out sides, ends, and cupola sides and ends from Evergreen .060" scribed sheet styrene.

I made the windows for both the sides and the cupola side 1/2" wide by 5/16" high

I cut the AF cupola in the middle horizontally, then added scrap stripwood to raise the top by 3/8" (two scale feet).

The most difficult parts to create were the ends of the cupola. I used paper to make an accurate template. I then trans-

ferred the pattern to the sheet styrene. The four end windows are smaller than the side windows, so I made them 5/16" x 5/16" square. I cut window frame strips from Evergreen strip styrene. This helped square up the windows nicely as my carpentry skills with a hobby knife are adequate but not terrific!

I capped the sides with Evergreen No. 294 1/8" L angle. I also added an Evergreen No. 126 .020" x .125" styrene strip on the top of each side; this is where the SOO LINE Script is placed on the finished model.

I filled the joint gaps where the cupola sides and ends meet with some Testor's model putty. (Better craftsmanship may require less finish work.) I also used the Testor's putty to fill in the gaps from cutting the roof.

After finishing the carbody with primer, then touch-up, then painting and decals (heralds from an old Miller HO caboose set), the new body was ready to reinstall on the lighted No. 930 frame.

I modified the end rails by removing the left post from each railing set, thus duplicating the original ladder style.

Then I added a T smoke jacket in the original chimney hole.

Adding the metal cupola bracing and window glazing will be nice final touches.

PHOTO 2: In this photo, a razor saw has already been used to cut across the roof just to the right of the cupola. The left-hand edge of the masking tape will serve as a guide for the second saw cut across the roof. Once that is done, repeated scribing with a hobby knife along the top of each side between the two roof cuts will complete the removal job.

PHOTO 2

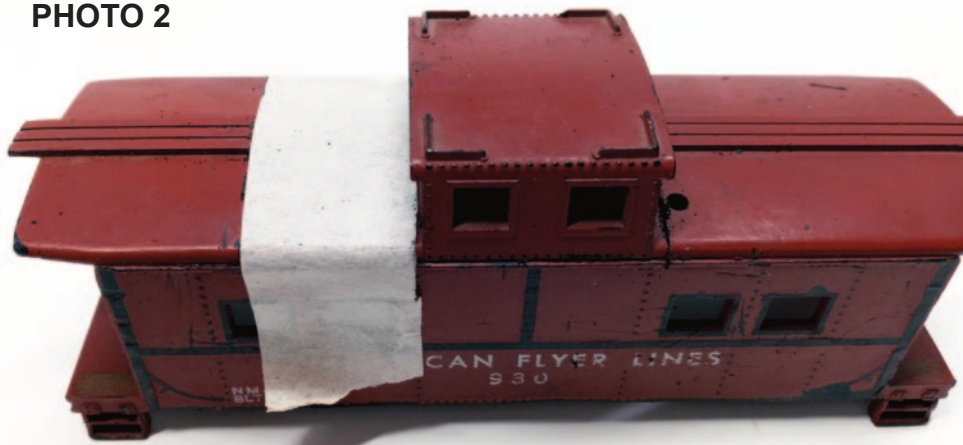


PHOTO 3

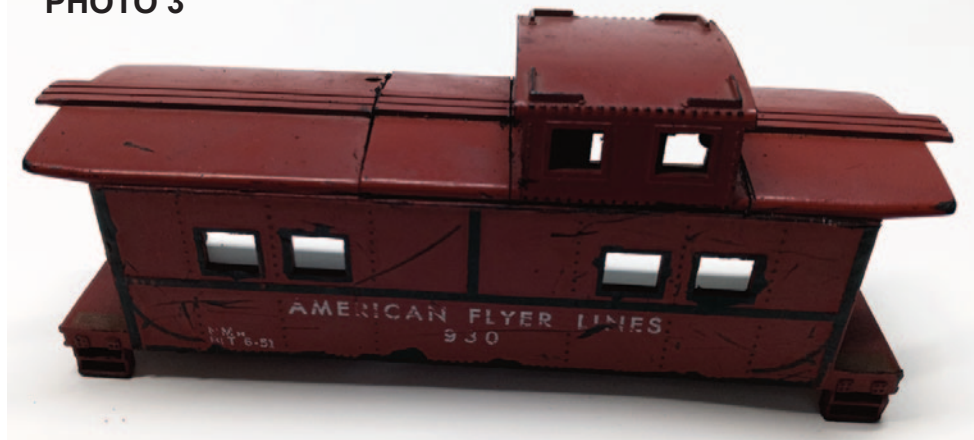


PHOTO 3: Here's what the carbody looks like with the removed roof section reversed and glued back in place.

PHOTO 4: Here we see the cupola cut in half horizontally. Yes, I know the photo belies the text. I did this before realizing that I didn't have to separate the cupola from its adjacent roof section. If you use my article as a guide, you will have a roof section integral with the bottom half of the cupola.

PHOTO 5: I used chunks of stripwood to properly space the cupola roof above its base before applying the new sides and ends.

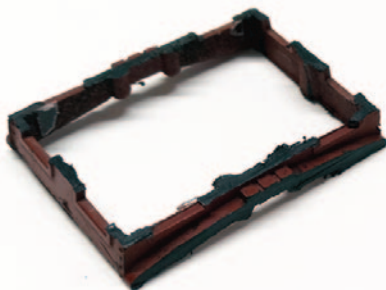
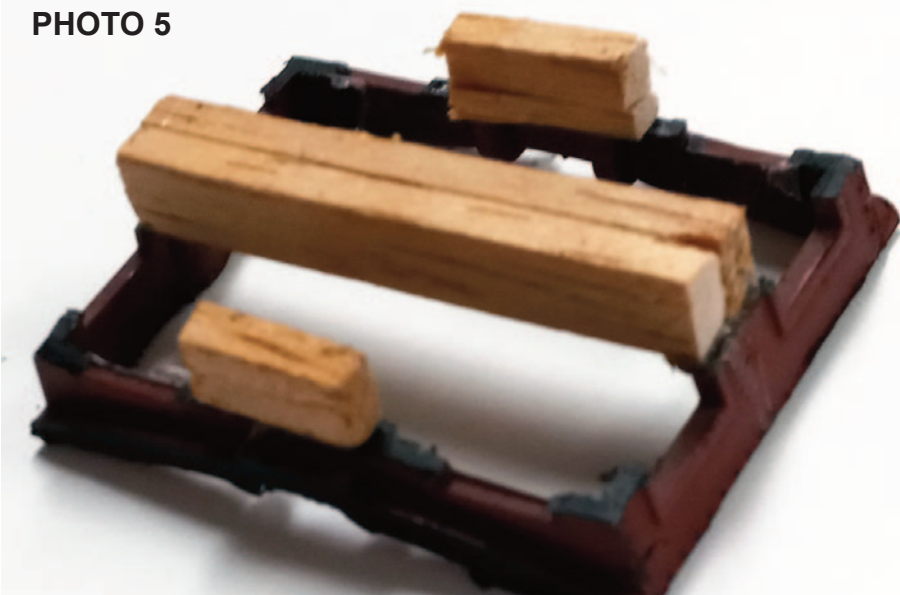


PHOTO 4



PHOTO 5



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A tricky problem

Solved by a simple lift-out building

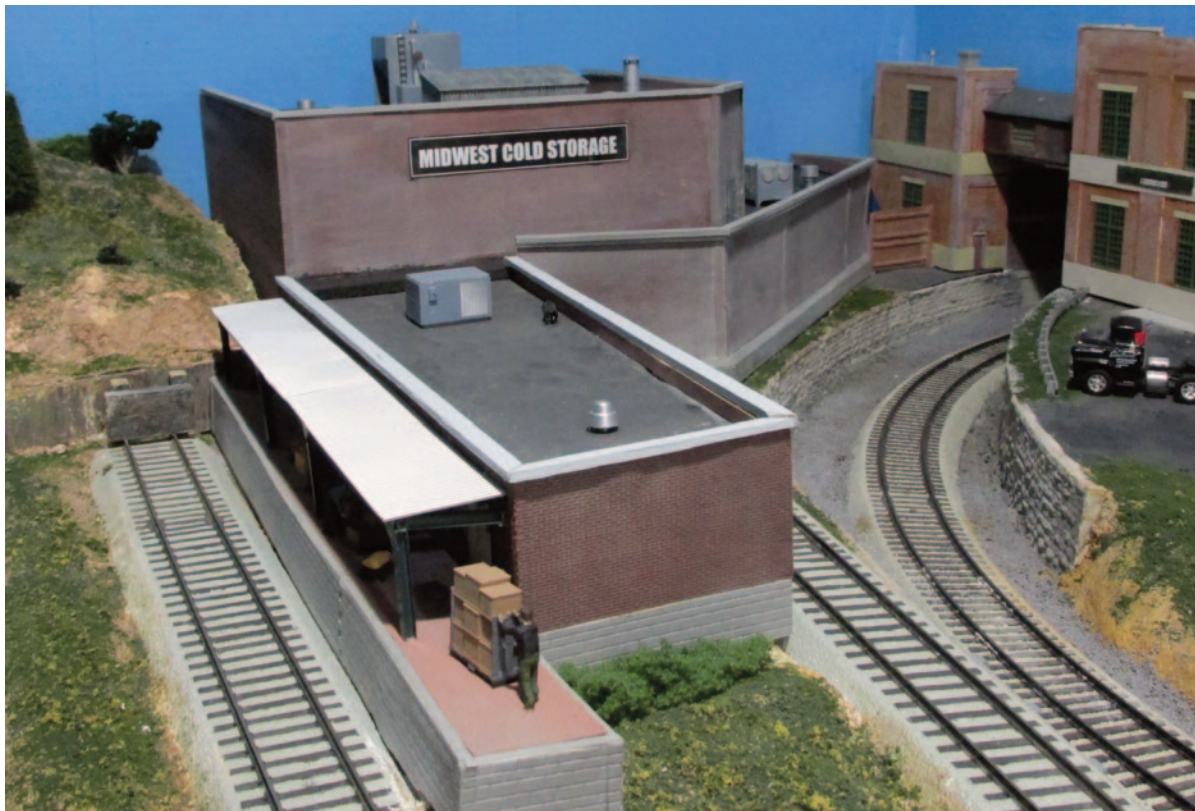
**Gerald Stebbins,
West Branch, IA, photos
by the author**

It was always in our plans to move to a new community after my retirement, and for several years prior to the “big event” I considered the parameters of a new layout that I would be constructing in our future home. One of the things I considered a must-have was a scenic divider that would separate the layout into rural and urban themed areas. I also planned on an upper and lower level, of which the lower track would be partially concealed under the scenery; this would provide the layout with a hidden staging area. Of course, the nitty gritty details of the design would be driven by the constraints of the space available in our new house.

Fast forward to the present, and my retirement is now a fact; we have relocated to a 1960’s ranch-style home in the small community of West Branch, Iowa. One of the selling points of the house for me was that it already had a finished basement rec room, with ample space for the layout I had envisioned.

As ultimately constructed, my layout now consists of a 15x6 ft central peninsula, with a lateral divider going down the middle of the long dimension, that terminates on the “south end” in a ceiling support column. At that point, the layout continues beyond the column, and the track loops around from one side to the other through a simulated river valley.

However, on the “north end” of the layout, the divider runs all the way to the basement wall, so the main-line tracks must go through the divider at two separate locations.



The “east side” of the layout is the rural environment, and it was easy to disguise the pass through the divider by running the track into a couple of tunnels. But on the “west side” -- or city side -- of the layout,

I did not want to employ the same device. There I settled on the tried-and-true method of using structures to hide the transition.

Complicating matters, I had expanded on the original track design to incorporate a

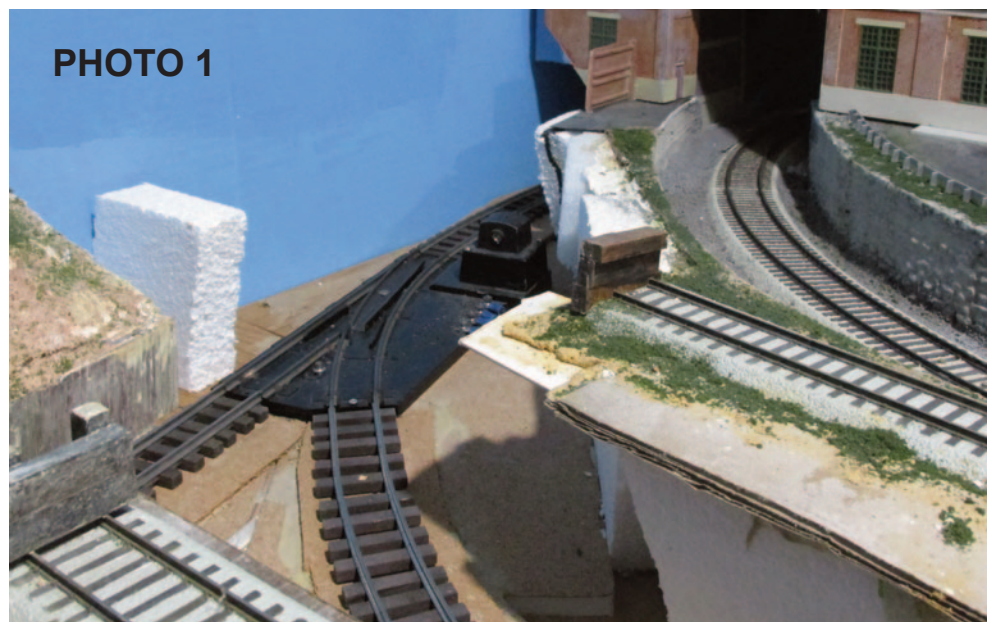


PHOTO 1

branch line. I had added additional benchwork running against the wall on the west side of the layout. This necessitated adding another turnout in a most inconvenient location.

As Photo 1 shows, the turnout landed right next to the place (on the north side of the layout) where the hidden level of the main runs through the scenery divider. Obviously, I had to make a provision for maintenance of the hidden turnout, as well as having access to deal with any derailments at that location.

The solution for both of those problems was to design an industrial structure large enough to disguise the turnout location, which would also be removable – a lift out in other words. It would have to be light enough for easy handling, but rugged enough to take the abuse of being periodically removed and reinstalled.

I decided to build a cold storage facility. I chose this industry for two reasons. First, I have collected a number of refrigerator cars, so a cold storage warehouse made sense from a traffic generation standpoint.

And second, as a rule, cold storage facilities tend to have solid walls that avoid windows and doors, except where necessary. This latter point was also in keeping with my plan to build the structure with a completely solid interior for strength and stability.

Photos 2 and 3 show how the main part of the structure was formed using blocks of stacked ½ inch Styrofoam sheet. The first layer of foam was installed as two separate sections, with a path through the center that aligned with the lower track to provide clearance for trains to pass underneath.

The building's irregular shape is dictated by the visible tracks running next to it. I finished the exterior of the structure with S-scale brick sheet material from Monster Model Works. I painted it with Floquil Caboose Red and finished with a light grey acrylic wash to pick out the mortar lines. Lengths of strip wood painted a concrete color add additional detail.

The roof is black painted sandpaper. The A/C units and ventilators come from HO rooftop detail parts sold by Wm. K. Walther's Co.

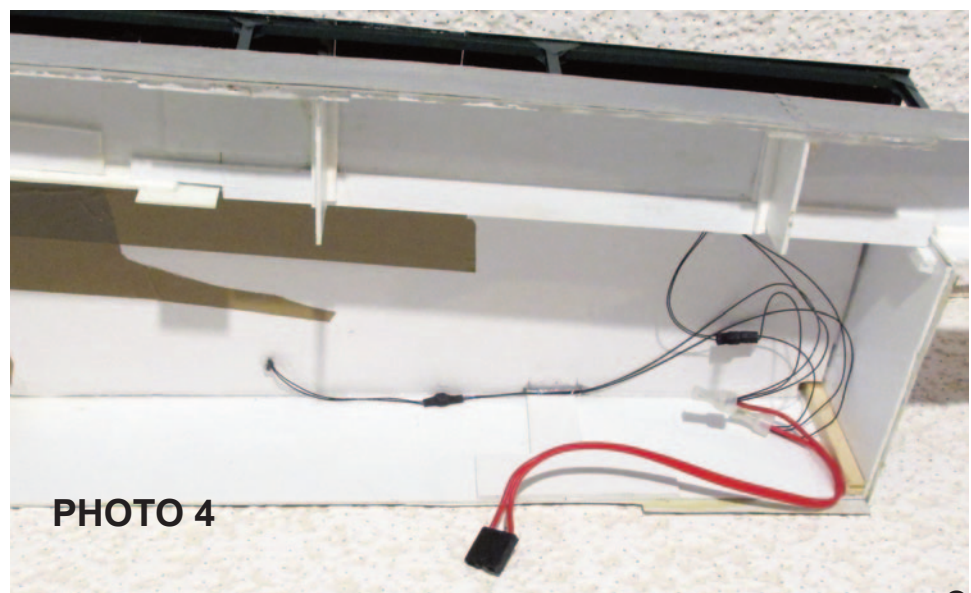
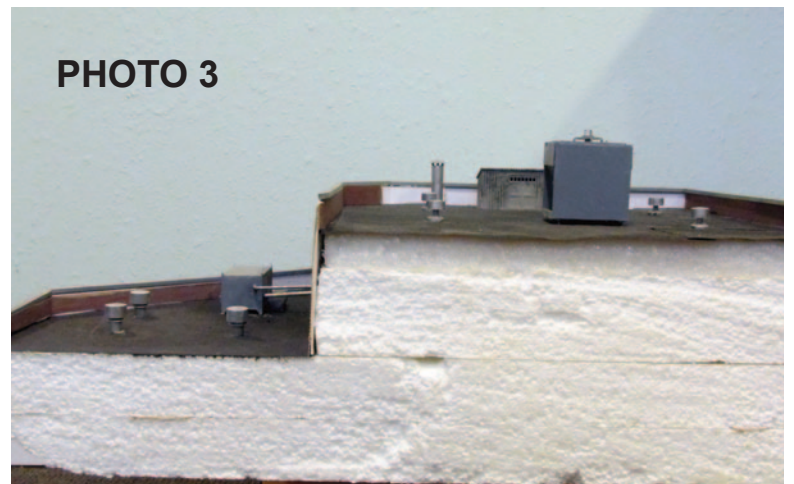
Of course, to service my reefer fleet, I needed a loading dock. I built this out of styrene as a separate element that attaches to the main structure. I made provisions for interior wiring, including LED lights over the loading dock doors, and a spotlight on

the roof that illuminates the sign on the main building.

There is a quick disconnect for the lighting circuit for those occasions when I must remove the structure from the layout.

The awning over the loading dock is built up using Plasstruct beams and corrugated sheet material. I also used the same material for loading dock doors. I was planning to make a third loading door on the backside, but that side is virtually invisible from the aisle, so I just park cars on the back spur and let the viewers use their imagination.

The docking area is detailed **See Problem on page 21**



CREATING:



Empire State Express 999 from an American Flyer Frontier Set

Michael Byle, photos by the author

I had an old broken Franklin engine and scuffed up Frontiersman baggage and passenger cars sitting in my shop. The loco had a bad smoker and a broken roof, but otherwise ran well.

Since I am from a New York Central family, I thought they might enjoy a new life as NYC Empire State Express. The Franklin Atlantic engine bears a reasonable resemblance to the 999 engine that held a passenger rail speed record at the turn of the last century (Photo 1). While not an exact replica, it seemed close enough to do the way A.C. Gilbert would have done it.

Accordingly, I set to work on making minor modifications that would pull off the effect I was going for. The most remarkable difference between the two is the smokestack. I set down a scale drawing of the smokestack I wanted, but found no simple way to make it until a friend of mine told me he could make me one in ten minutes on his 3-D printer.

It came out perfectly, so I cut off the old one, reattached the broken piece of the cab roof and got to work on the rest (Photo 2).

I had to put in a couple of braces to support the roof that after painting all but disappear (Photo 3).

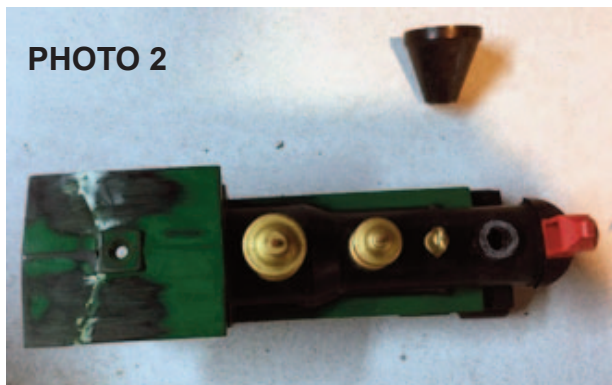


PHOTO 2



After repainting the shell in a couple of coats of black paint to cover the red and green base colors (Photo 4), I went about creating the lettering the same way A.C. Gilbert did with the original. I printed the

wheels black. I highlighted the wheels and body with silver striping and replaced the brass handrails with new ones made from stainless steel wire.

I added a jewel to the headlight to im-

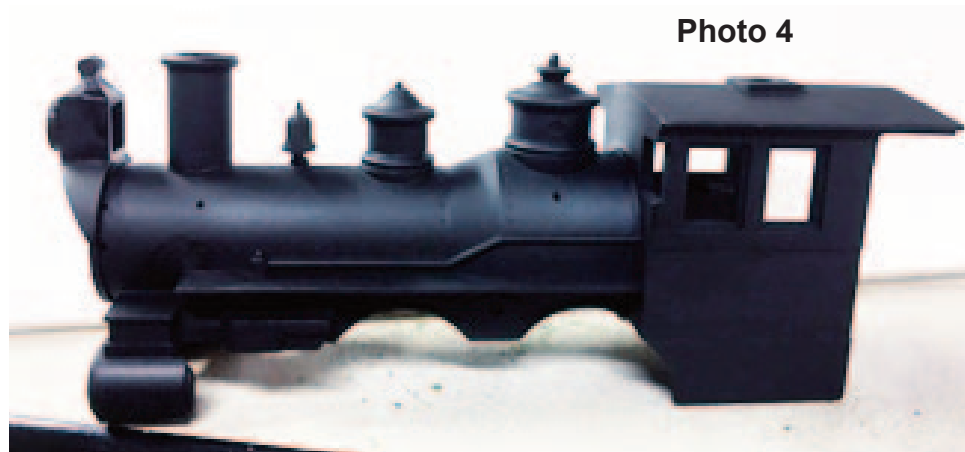


Photo 4

lettering on label stock and stuck them on.

I did use a clear coat over the entire engine and tender shells for a glossier look. I rebuilt the smoker, painted the

prove on the foil of the original and added an engineer in the cab.

I also added black highlights to the running gear. I completed the silver striping using a fine tipped paint pen. I painted the bell using a gold paint pen. I then painted and highlighted the wheels, and rebuilt the smoker using a kit from Portlines.

Once that was done, I took two of the cars and repainted those green with black roofs and added gold NYC decals.

I enhanced the look by creating silhouettes in the windows of both cars by printing them on film and backing them with paper (Photo 5).

I replaced all of the handrails and rods

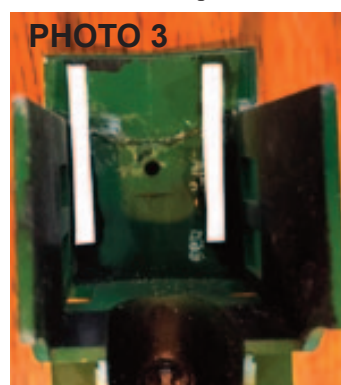


PHOTO 3

with new ones formed from stainless steel wire.

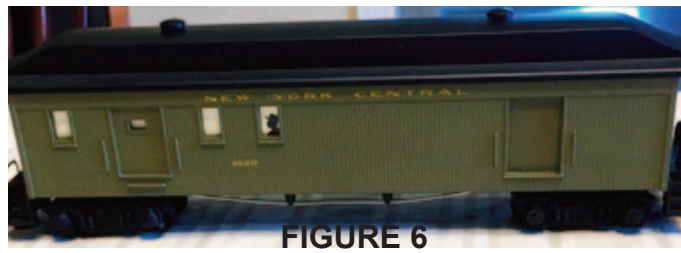
I really disliked the open doors on the baggage car and made doors from scrap styrene scribing them with a hobby knife to

approximate the wood look of the car (Photo 6).

I also thought it would be interesting to add silhouettes in the ends of the cars.

I am not a rivet counter, but I do enjoy creating some-

thing from old discarded components. It is rewarding to bring new new life to what might otherwise be trashed or dismantled for parts. In the end, I think it does look like a set A.C. Gilbert might have produced.



**View
of the
completed
Empress**



Quick Tip

Make your own simple end of track stop



Andrew Lewis, Elkins Park, PA, photos by the author

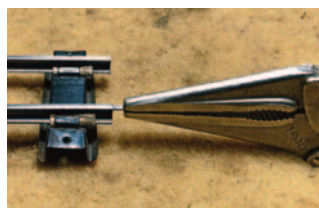
I have an American Flyer layout and use the original Hi-Rail Flyer track. Since it is a compact layout (6' x 12'), I mostly have mainline running. Although, I do have a few shorter sidings.

Like most Flyer modelers, I went to my track surplus and pulled out those A.C. Gilbert green bumpers, which do light up well.

However, I noticed that they assume a bit of track space when you have a short siding.

idea of a "chock" came to me and with an amazingly simple idea, it looks great. This will best work with hollow rail, which uses metal joiners.

1. Take the existing joiner out of the rail with pliers. Be careful, as sometimes the joiners stuck tightly in the track.



Since I did not need bulky green bumpers, with the big, bright red light, I thought of going with something less bulky and more utilitarian for my siding.

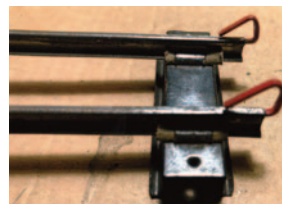
So, not using any photos or prototypical drawings, I just wanted something simple to stop a car from going over the edge of the stub siding. The

2. Use a needle nose pliers and make 2 bends to form what looks like a triangle with your joiner. You can use a flat surface when bending it. Leave a slight gap to make sure the joiner can slide into the end of the rail. Make two of them.

3. Take red, yellow, or orange paint to brush or spray on both joiners and let dry. If you plan on inserting them permanently, you can add glue to the ends and slide them into the end of the track.

Disclaimer: This may not "STOP" your trains if you are not paying attention, but it should keep any loose cars from going over the end of the track,

For me it gave a more streamlined appearance, and freed up a few inches, which in a short, stub siding, gave me a little bit more room for a longer car or extra uncoupling room. Yes, there is no light, but in my situation, they look better with certain sidings. I hope this was helpful to you.



Upgrade farm scene to include a realistic silo



Bob Davis, Old Lyme, CT, photos by the author

For many modelers: placing a barn, some cows, a tractor and a Plasticville house on our layout was all we needed for our farm scene. However, something always seemed missing and that was a silo to store the animal feed.

The good news - silos seem to come in all shapes, sizes and construction material. They store the animal feed over the winter but also require a mechanism to load the silo. Both of which I tried to incorporate along with a haystack for fun.

My first attempt was to use a piece of PVC pipe with a 2" inside diameter 7-1/2" long and with a hacksaw and Dermal cut grooves resembling a block silo that I had seen often. Adding a piece of half round carved Styrofoam for a roof cap, gray primer for grout and we had a place holder known as silo 101.

When I later found a picture of a wood sided silo and feed auger - I decided to upgrade.

Silo

You can use coffee stirrers or craft store wood strips. I cut them to make 2 or 3pcs to cover the original PVC silo - your choice on how many staggered joints. (Fig 1)

I use Goop contact cement that is tacky and will not squirt through like white glue. I alternated the pattern and surprisingly it

was a perfect fit with the last row (Fig 2).

I then wrapped wire around the wood strips for looks, adding more rings at the bottom for support since that is where the weight would be from the stored feed. By staining the wood, it also darkened the wire in one step. (Fig 3)

If you choose to paint the wood you will need to color the wire a rust stain or let it rust prior to wrapping the painted wood.

Auger

Again, you can select any style to lift the feed up a tube and load silo from the top. I made my auger shed out of wood with cardboard roof cut to follow the silo curve. The building itself is 1-1/2" square by 2-1/2" high (Fig 4).

I used a 3/8" dowel cut at various angles to fabricate the conveyor chute (Fig 5).

Again, what ever looks proportional as long as the chute reaches the roof. I was told they would draw down the feed from a silo door that opened within the barn. You can notch the barn roof so silo fits flush to the barn to be more precise.

Not me, the American Flyer barn is a hard piece to find.

Silo Roof

From pictures it looks the silo roof is metal with crimped edges. That was too much work, so I glued pieces of wire onto the roof to give a ridged profile and applied spackle to mold over the wires giving a smooth transition.

Next apply a coat of paint and then attach the conveyor chute. (Fig 6)

Haystack

Looks simple but finding straw like material that can bend was difficult. I made a Styrofoam cone and using Goop, glued on thin pieces of straw. After that, each follow-on layer was built up using spray glue. Add plastic figures and the scene becomes alive. (Fig 7)

Hope this gives you ideas for your farm scene without a lot of detail modeling. My approach is to put the right pieces in a scene with proper perspective, giving it life and understanding to the train room visitor. We all know very few care whether we have a 3- or 5-digit box car but they immediately go to the layout - righting any fallen cows & animals, so a complete farm scene is essential



Bob Davis is a member of TCA since 1976, NASG since 2005 and current member of the CT-S-Gaugers club. I am Hi-rail with an emphasis on modeling - depicting coal mines/tipples, engine facilities, circus, logging areas, farms, rural as well as city life.

Layout is 24' x 30' with 3 levels and 13 sidings all filled with trains ready to run. I operate Flyer but lately have moved into American Models, S helper and TMCC engines from Lionel with two Mikados, a Pacific, a Challenger and Mallet among the latest acquisitions.



PHOTO 1



PHOTO 2



PHOTO 3

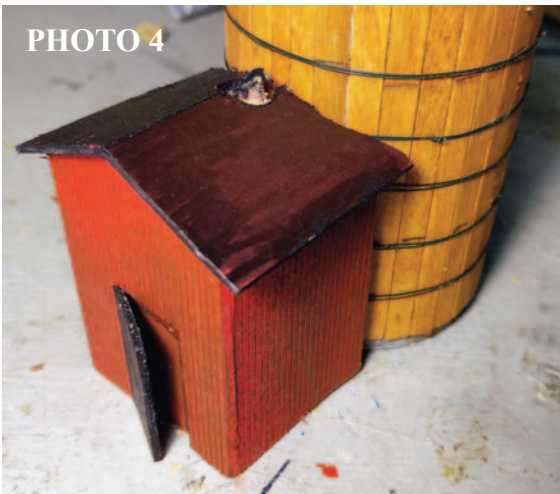


PHOTO 4



PHOTO 5



PHOTO 6



PHOTO 7

Plasticville

Don't neglect this inexpensive, readily available product

Jeff Madden, Wales, WI, photos by the author

Whether scaler, hi-railer or tin-plater when working on a layout it is a promising idea to take a look at vintage as well as current structures and accessories such as those Plasticville offered by Bachmann.

AF layout builders have always been attracted to Plasticville items to quickly set up towns and villages. Most Plasticville buildings were a bit undersize for O, and a bit oversize for S, so both S and 3-rail O gaugers in the day were delighted to be able to create villages with the convenient snap-together pieces that were pre-painted and decorated.

Several other companies that mimicked Plasticville type structures included Littletown, Marx and Skyline. Some Littletown buildings and accessories were reissued by Bachmann when they took over the line in the mid-fifties. The Mansion House is one example of structure that was continued by Bachmann. Most Littletown's buildings had slide connectors instead of the dovetail interlocking of Plasticville. Marx and Skyline also produced snap-together type structures. K-Line eventually took over the Marx line.

If you want the history of these type of buildings and accessories check out the softcover book by Bill Nole *Plasticville USA, Classic Guide to vintage O Plastic-*

ville. Hmm, S is not mentioned in the title. It has four color pictures of the Plasticville items and includes references to Lionel Plasticville, Storytown and Make 'n Play. Not included in this latest price guide version are pix of Littletown, Marx and Skyline.

AF modelers discovered that Plasticville type buildings could fill the bill to populate layout space in a speedier fashion than building up the various wood kits offered. Houses, stations and commercial structures were found to be highly useable.

I, have kit-bashed numerous plastic structures in my HO days, at once eyed the possibilities of using and modifying Plasticville type buildings. I quickly discovered that complete kits, built ups and parts were easily found (usually inexpensively) at the various swap meets.

The type of structures I sought were ones

that would fit in my rural West Virginia theme layout. At the meets I picked up a diner, a Frosty Bar, a covered bridge, a tower, outhouses (I cannot resist them), some signs, telephone poles, etc.

As to gas station needs (all layouts need at least one) I gathered up a couple of the smaller Plasticville ones, a K-Line one (ex-Marx) and a rare Littletown one. As to the smaller Plasticville one, I liked the art-deco design, but I discovered the depth of the bays were too short for a 1/64 car. So, I bashed two together by taking the back off one and the front of another to double the depth. I displayed that concoction at the 2017 NASG convention along with a modified diner. This chrome one received a



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black roof, roof vents, a new door, an interior and new signage.

Yes, the common dairy barn got an extension (1/3 longer), a 1/2 inch stone base and a re-paint for use in a farm scene



which includes a Rail-Town (MTH) farmhouse (they are undersized for O too) and several Plasticville outbuildings. Of course, with a red and white farmhouse there had to be a red and white Plasticville outhouse



to complete the scene.

More on the outhouses – I model the 1950s where outhouses in rural areas still existed, even on the real railroads at remote locations. Yes, I do use ones by other manufacturers too, but those Plasticville ones are just too cute to pass up. I even made one-holers out of a couple for a better look by modified Plasticville bungalows that are used in my company lumber town. Other 2-holers get a re-paint to match the buildings paired with them.

Back to the gas stations – The rare Littletown one was kept as is except for a stucco paint job on the plain white sides, a black roof and new signage. The raised lettering (Littletown) was covered over by a thin strip of styrene with new wording. This trick also works for covering up



the raised Plasticville lettering on their buildings. A K-Line gas station was changed by shortening the lengths and cutting out one window section of the bay doors. The Plasticville Colonial Church found a place along Elk River Road.

Other Plasticville items will also find a home on the layout as scenic work progresses.

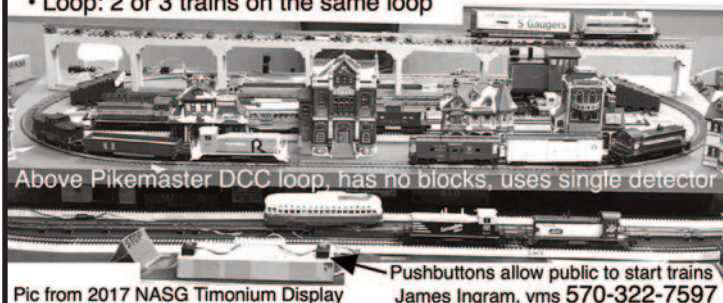


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Christmas 1947

Getting your first American Flyer train



Jerry Rivet, The Villages, FL

It was Christmas Eve 1947, and my mother decided that I who was seven and my younger brother aged four along with my dad and grandfather (known as Pepere) who lived with us would attend Christmas Eve services. At the French-Canadian Church.

We were going to meet my grandfather's brother Ferd and his wife Andrea there, and all attend a French Canadian Reveillon afterwards. This is a Christmas eve tradition practiced in Quebec Canada, as well as in France.

We had to walk, as we did not own an automobile or truck. As luck would have it, it began to snow after we had traveled two blocks on the flat, and the hills were just ahead. It was cold, windy, and most of all only plain miserable out.

We finally made it to the entrance door to the church, shook of the snow-covered winter coats and entered the church. Then Pepere walked us to the correct aisle to seat the

family. There seated in front of us was his brother, and his sister-in law.

The Christmas concert lasted for fifty-four minutes, and I am positive of that. For two reasons, my pep-ere and my uncle, who was also my godfather, both wore rail-road pocket watches. I know we are finally getting to trains. I cannot count how many times we went to observe the real honest to goodness trains go steaming by, while we all sat on the hill near the double main line, one heading north into New Hampshire, then onto White River Junction in Vermont, the other went south, towards Boston. Auto-mobiles, and trucks, were a scarcity back then, railroads were the mode of travel

After services ended, we met Uncle Ferd and Aunt Andrea and began our trek to their house for the Reveillon. It was snowing, cold, windy, and difficult to walk, nothing had been plowed yet. When we arrive at the corner, and my dad and Pepere were to go home and gather the food that mom had prepared for the celebration. It was a slight hill, and my

younger brother was crying and being fussy, due to the harsh weather conditions. Well uncle Fern picks him up and carries him all the way to his farm. Not me, I walk with Mom holding one hand, and Aunt Andrea, holding the other.

My uncle put my brother down, to be able to hold his hand, and not fall while attempting to negotiate the slippery brick stairs. We were on the left side off the farmhouse, looking at it from the street side. And the hill was still going up.

The first thing, Uncle Ferd had to go in the basement looked like a tunnel to it and retrieve two split wooden logs to keep the wooden kitchen stove and the combination great room and dining room pot belly stove going.

Then my brother and I received our first Christmas surprise for in the dining room was an American Flyer, green passenger set. The set ran under the dining room table, in a figure eight.

My brother and I lay down on the floor and watched it go round and round. The new

radio was playing, and the Christmas tree was decorated and lit. Now we waited for Pepere and Dad to return. What more could one ask for, warm house, huge tree, and food on the way.

My brother and I were enjoying the American Flyer, black steam engine pulling a green passenger coach set. Round and round it went, smoke coming out of the steam engine stack as it traveled in a figure eight. As Pepere said, it was going through tunnels, no bridges to span on this railroad line. There were also stations at either end to stop at.

It was time to go, but what really happened was that my younger brother and I had stuffed or stomachs' full of food and we were tired. We went back to the trains, but with full stomachs staying up past our normal bedtime, playing trains and we both fell sound asleep. The elders decided that putting a blanket over us was fine. When I woke up the next morning, my brother was in the window facing the walkway, where Uncle Ferd was already shoveling snow. Mom, Dad, and Pe-

pere had left during the snowstorm.

We were going to ride in Mr. and Mrs. Carrigans horse drawn delivery wagon, that could be adapted to a coach style wagon. The snow was deep, as uncle Ferd had estimated that we had received seven inches of snow.

The team of draft horses had no difficulty in the snow, and the Carrigans were only going to dine, about a mile or so from our farmhouse, then return to our farmhouse for dessert. I still love Christmas, trains and parties. We left my godparents house at about 11 a.m. Christmas day.

It was only about a quarter of a mile away. It was a warm, cozy open wagon ride, with my brother tucked in between Aunt Andrea and Mrs. Carrigan. I was wrapped up in a protective heavy duty shipping blanket, next to Uncle Ferd. We were both snug as a rug.

When we arrived Dad, and Pepere, were busy shoveling a spot for the Carrigans wagon to reside, while they were spending Christmas day with us. We were dropped off, and like a bolt of lightning my brother was running to the back porch, he was all excited to see whether Santa had come to our house, were there any homemade chocolate chip cookies left, and finally was there a



glass of eggnog still on the table.

But we had chores to do. Go and each one of us, gather two split logs of firewood for the kitchen stove, as well as the family room stove. That is where the Christmas tree was located.

Were Dad and Pepere surprised when they saw wood near the stove. Mom won the men's hearts with that gesture, for sure.

After eating Pepere went and sat down in his favorite rocking chair and was soon fast asleep and Mom called to me, it was December 25, and that was an odd numbered day. That meant in our house, I washed

the dishes and my brother dried. I did get a break though; Dad would wash the pots and pans. Then and only then, would the Christmas presents be opened.

It now comes the moment, that my brother and I have been waiting for. The opening of the Christmas presents. Since Pepere was the eldest, he opened first.

Well, Pepere opened his box of goodies and inside was a red knitted hat, two steel tree wedges, a pair of deer skin mittens and a metal container with a handle on the top of it, that went back and forth, with an electric cord attached to it. (A transformer in today's vocabulary.) Also in the box was three sections of American Flyer real two rail track.

Then it was dad's turn to open his box containing a red knitted hat, a pair of deer skin mittens, three pieces of AF track. Then in the bottom of the box was a brown corrugated cardboard with an Atlantic 4 4 2 steam engine wrapped inside. We now had a transformer an engine, what was yet to come.

Mom comes out of the kitchen and opens her box. No red knitted hat, but a snow white one, with ear flaps, and two cords to tie in a bow beneath her chin. Also, she re-

ceived an ever-famous American Flyer RED caboose with a # 690 track terminal.

I was next, so I go for the largest package, and to my surprise no train stuff insight but a knitted blue hat next a pair of yes leather mittens. Then I look in the bottom of the box and finally two curved tracks, a small roll of black stranded wire and a dark green gondola.

Then it was my younger brothers turn. He received a pair of leather mittens just as I had. Also he received two curved tracks, a small roll white colored stranded wire and a Tuscan red box car. So now we had a train set.

We needed a book on how to assemble the train set and to our rescue arrive Mr. and Mrs. Poirier who operate the hobby shop in the Farmers Exchange.

Who arrives next but the Carrigan's who had brought, not only a sweet treat but four sections of straight track. Now we had a railroad empire. We set it up around the tree.

The model train is running around the Christmas tree, and Mom invites all into the kitchen and we all overindulge in the "sugar high" Christmas feast. Pepere recites grace and announces, "The trains are on time, so let us eat. Merry Christmas to all."



Tom Hurd

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You can go home for Christmas



PHOTO 1

Joseph A. Tringali, North Palm Beach, FL, photos by the author

It was Christmas, 1948 and I was three years old. My cousin John (who lived upstairs and was everything I ever hoped to be) was eight and a half years old. All I got for Christmas were stupid toys.

John got an ELECTRIC TRAIN! It was the American Flyer 3-car New Haven set. The cars were S scale with wide trucks that ran on three rails the size of Lionel.

I can remember watching John run that train around under his Christmas tree every year, while strands of shiny lead tinsel fell and melted on the rails.

Time marches on. Eventually John outgrew the Christmas tree set (thank God that did not happen to me) and gave me his train! Wow! It was great for a while but then my parents got me a brand-new 1956 Lionel Union Pacific passenger set (the gray over silver one), and the AF became, well, you know.

Although it held a lot of happy memories, I passed it down to our younger cousin Alan.

Time marches further. When I was married and in law school my wife and I visited the aforesaid Alan and I saw the Flyer set—locomotive, cars, tracks and everything—tossed randomly into a bushel basket

and “stored” in his parents unheated, detached garage.

I politely asked for the return of the set. My request was granted with an offhand, “Oh, that? Yeah, sure!” That made me think the basket might have been intended for the front curb.

The locomotive was a total loss, but the passenger cars were viable. My first trip was to a local Buffalo legend—Spoonley the Train Man, where I asked him to replace the three-rail trucks and their link and pin couplers with appropriate two-rail trucks with knuckle couplers.

Chester Spoonley was dead set against that idea, telling me that I would destroy the value of the cars. I responded that I did not care as I intended to keep them for the rest of my life. Any loss my estate might suffer would be more than made for up by the pleasure I would get from watching them go round and round and thinking about those happy days with Cousin John.

Time marches ever onward. I began my career as an Assistant

District Attorney in Buffalo and had a nice “train table” featuring the Lionel set in the basement of my home.

A severe heart attack at age 39 got me out of the stressful trial lawyer life and triggered a move to Florida where I became an Assistant Attorney General and spent my time doing legal research and writing appellate briefs. It was a good, quiet life, but I needed a train!

Now, here is the thing: after you have had a serious heart attack and a septuple bypass (yes, that means 7) your spouse will let you do darn near do anything you want to do if there is no stress involved.

We did not have a basement or attic, but we did have a 7 by 10-foot ell off the living room, where I was allowed to build a 5 by 7-foot train table. When you are an adult you have something called a credit card and you do not have to save up birthday and Christmas money to buy train stuff!

Like every other model railroad guy, I had been collecting things over the years and because of my early childhood memories I bought more than my fair share of Flyer. Now with a new beginning it was time to take the final plunge. I sold the Lionel equipment (except for the “keep-sake” Union Pacific passenger set) and went full speed ahead with American Flyer.

My new pike had four immutable laws: (1) I would intentionally keep the look of a toy train but would make the scenery as re-



PHOTO 2

—thanks to American Flyer

alistic as possible; (2) I would use as much of my accumulated Plasticville stuff as I could for old times' sake; (3) because of limited space, I would use every square inch of the layout, including the corners which always seemed to be wasted; (4) finally, and most importantly, the layout would tell a story that would relate to my life.

So, I set the scene in 1952 which allowed for the changeover from steam to diesel. Christmas meant there would be lots of snow (You are heard of Buffalo, right?).

The Plasticville country church was dressed up to look like Our Lady of Perpetual Help in Lake View, the church where I grew up, and became Our Lady of the Rails.

The Plasticville Television station became WPLA Radio for the World's Perfect Listening Audience which broadcast Buffalo's own Clint Buehlman every morning for news of school closings.

Plasticville's Frosty Bar became the iconic Ted's Hot Dogs, and the General Store became my father's New Era Market—complete with him standing out front in his white apron.

So, any time of the year, regardless of the Florida weather, I can sit down in a comfortable chair, turn down the living room lights, and go home for Christmas.

My Buffalo of 1952 may be gone forever, but it will always exist in 35 square feet of paradise in North Palm Beach, Florida. Stop by anytime and I will be happy to give you a tour of my miniature hometown.

PHOTO 1: When you have only 35 square feet, you must use every square

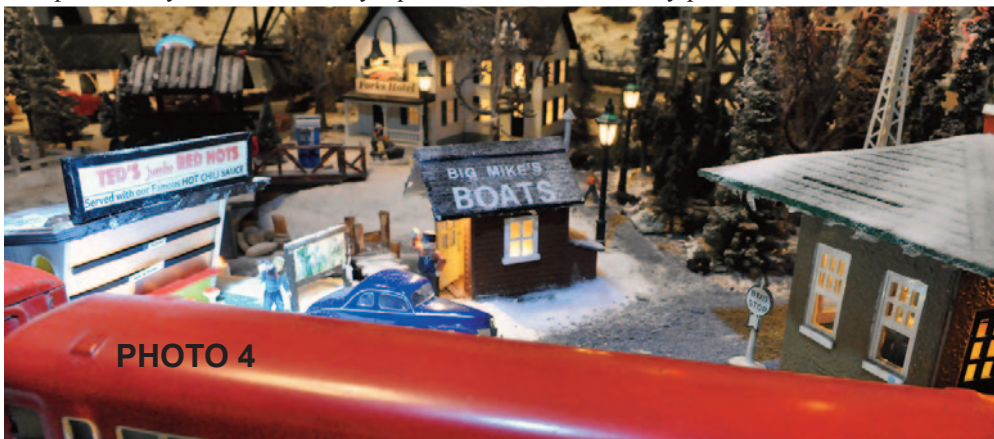


PHOTO 4

inch. Besides, I really enjoy making scenery.

PHOTO 2: That is my dad, Sebastian Tringali, standing in front of his New Era Market as a last-minute shopper leaves the store. "Never close the door on a customer," he used to say to Mom whenever she complained about him being late for



PHOTO 3

supper. I made the "New Era Market" sign from memory, using a computer, and Pepsi Cola caps from the Internet.

I found Buehlman's photo on the Internet and make the billboard. The station letters could not be changed without wrecking the Plasticville building. The Buffalo Creek boxcar was specially made for me by my friend, Bud Rindfleisch of Hamburg, NY.

PHOTO 3: The railroad's new Silver Flash diesel locomotive is heading The American Flyer Line's crack passenger train, suitably painted with a Christmas

tree for the holiday season.

Ma Bradley's Riverside Café is imaginary. I was adopted at birth; my birth mother was Mary Bradley, and her mother, Evelyn Bradley, was a cook. I figured Ma Bradley, should get a mention in my life. The Television Every Nite sign is placed so that radio personality Clint Buehlman must

see it every day from his parking lot across the street. Clint refuses to eat at Ma's place because it is a greasy spoon. Ma intentionally had her boys hang the sign up crooked. She told them, "That'll drive Buehly crazy!"

The tow truck and police car are a little reminder of my first criminal prosecution. Hans Schmidt was an eccentric millionaire who loved riding around in a large wrecker and would pull up in front of a local café, turn on the flashers and go in for a meal. Everybody knew Hans and thought it was great fun until A new police officer decided to write him a ticket for blocking a thoroughfare. Hans tore up the ticket and threw it on the ground, so the police officer wrote another ticket for littering .

Eventually Hans was charged with a felony, and I, a brand-new (REALLY NEW) Assistant D.A. was assigned to prosecute the case. It was my first courtroom appearance and I was up against John W. Condon, Jr., one of the best criminal defense lawyers in New York State. After battling for hours, we eventually settled the case for a minor plea.

PHOTO 4: Ted's Hot Dogs is closing

You can go home for Christmas

for the winter, and Ted, Jr., is loading the car with hot dogs (Sahlen's—available only in Buffalo) and their special sauce for their relatives in Florida. I installed a new Ted's sign over the Frosty Bar and added pull-down shutters to close the place. Big Mike Zook had a fishing camp on River Road near Buffalo, and my uncles used to fish there. In the background is the Fork's Hotel where I learned to perform magic and became a member of Ring 12, International Brotherhood of Magicians.

Here is what the original Ted's looked like under the Peace Bridge between Buffalo and Fort Erie, Ontario. I constructed it from an old construction shed that was

used during the building of the bridge.

This is a close-up of the poster outside of Big Mike Zook's place. That is my Uncle Ben Pecorella with Big Mike on the left, and my Uncle Dick Pecorella and with my cousin Michael on the right. And yes, you can reduce photos down to that size on a computer!

PHOTO 5: Every year the American Flyer Line passenger train hauls in a "North Pole Express" caboose with Santa Claus inside for the area families. The "North Pole Express" remains on a siding next to Father O'Shea's church, Our Lady of the Rails, for an entire two weeks. By tradition, the family-owned barrel factory



closes (with pay) for the Holiday Season, and the caboose with Santa Claus uses its siding and draws families from everywhere.



Bench clean up leads to finished project

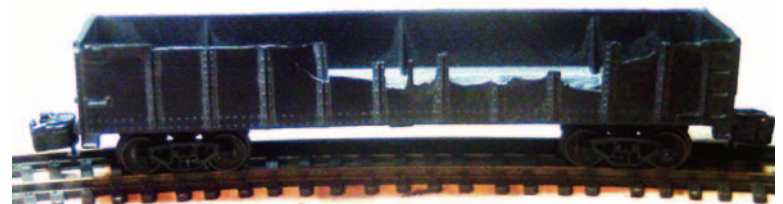
Bruce Weidner, Columbus, OH, photos by the author

As I was doing an organization and inventory of what was cluttering up my workspace, I happened upon some partially completed projects mostly rolling stock kits.

One is a failed cast copy of an S gauge gondola which I intended to weather up to model a very beat up gondola that had seen better days. While cleaning up, I decided I should at least add trucks to the gondola using assorted found hardware.

I did not have screws that would fit the mounting hole on the trucks, I figured that is why it was never completed. I did find a bunch of medium sized cotter pins. Well, they work well in this situation with a large washer with a small hole to keep the wobble to a minimum yet have enough play to swivel around on a curve.

Several test runs around a loop of track proved the idea worthy of further experiments. I found that in the case of this Flyer cast body that a shim (a washer) was needed to give the coupler



needed to not rub on the chassis (bottom of car).

This may not work in all cases but worthy of consideration





PHOTO 5

Lift-out building solves tricky problem

Continued from page 9

with workers, boxes, shipping pallets, and waste barrels that I obtained from various sources including Artisa and Model Tech Studios.

I find this addition to be an excellent solution that gives me ready access to a critical point on the layout, while simultaneously serving as both a visual anchor for one end of the urban scene and a logical traffic generator for my rail operations.

MEET GERALD STEBBINS

I grew up with American Flyer trains as a

boy, until I switched to N-scale in my teen years. I subsequently dabbled in G-scale and On30. I retired from John Deere in 2016 relocated to the rural community of West Branch, IA. Currently I belong to the Hawkeye Model Railroad Club, an HO scale club in Coralville, IA. It was a chance meeting with a Flyer vendor at a model railroad swap meet in my middle '50s that reintroduced me to the trains I started out with as a boy. Not being able to shake a lot of my old modeling habits, I have built my current S-scale layout as a hi-rail environment. I am a current member of the NASG, and have repre-



sented the NSAG at several local train shows with the NASG traveling layout.

GG AF Club offers billboard reefer sets for Flyer Fest West

Dale Smith, Secretary/ Treasurer

Golden Gate American Flyer Club is offering a set of Anchor Steam Beer billboard reefers with two different numbers as a set of two for \$160 plus \$15 shipping.

Anchor Steam® Beer derives its unusual name from the 19th century when “steam” was a nickname for beer brewed on the West Coast of America under primitive conditions and

without ice.

Flyer Fest West 2021 is being held on November 21 at Par 3 and Lodge at Poplar Creek Golf Club, San Mateo, CA. Registration is \$15. See our website, ggafc.org, for more details on the event and the cars, as well as a video about Anchor Steam Beer.

We have received a shipment of cars from Lionel and they are available for immediate shipping or pickup at our Flyer Fest.



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700 modelers display their work in St. Louis



James McAuliffe, Promotions Chair, photos by the author.

Seven hundred Model Railroad modelers gathered at the St. Louis Railroad Prototype Modelers Meet on July 30/31, 2021, to display their models and enjoy the comradery of fellow modelers.

Scale S modelers attending and displaying their models were John Mann, James McAuliffe, Sandy Sandberg, Mike Ostertag, &

Ken Zieska. Ken also featured models by Tom Lennon.

In addition to models on display, there were clinics with a theme of modeling Coal operations. Nineteen historical societies attended along with 55 exhibitors & vendors of model trains, electronics, parts, and scenery supplies.

It was great to see what fellow modelers have been doing for the last 18 months and exploring the learning Stations setup with live demonstrations of modeling techniques.



The top photo is an overview of the St. Louis Railroad Prototype Modelers Meet. Above left is John Mann with his set of scratch-built Wisconsin Electric coal hoppers from Stone Av. Station, LaGrange, IL on the CB&Q. Above right is Sandy James models diorama made by Sandy Sandberg presented by James McAuliffe. Bottom photo is Ken-Tom Models with models by Ken Zieska and Tom Lennon



Submitting Materials for Publication

The NASG Dispatch wants to publish layout articles and how-to feature articles from any NASG member. Good quality photographs that accompany the article are necessary for publication.

WHAT WE NEED FROM YOU:

A text file, MS Word or a pdf, of 1,200-1,500 words, prepared with no special formatting, typefaces, colors, or symbols. That text length particularly applies to layout features; some how-to features may be shorter or on occasion, longer. Some basic style considerations that you should keep in mind:

1. Use caps and lower case for all text, including titles and sub-titles.
2. Use only a single space after the period at the end of a sentence.
3. Spell check your text file prior to submission.
4. If you refer to an internet web site, be sure to include (and double-check) the full Web address.

If you are writing a layout feature, keep in mind that you will be providing a word-and-picture tour of your layout for your readers.

Tell them how your interest in S gauge developed and how the concept for your layout originated. Tell them about the things that you feel make your layout special. Include important technical details such as type of track used; minimum radius of curves; the types of power and control systems used; brands of locomotives, rolling stock, accessories, and structures.

Don't be overly concerned with writing style, punctuation, etc., we'll take care of that. That is what editors do. Just make it a personal and informal account, in your own words, of everything and anything you want a visitor to know about you and your layout.

A good selection of quality photographs or other graphics. For layout features, about 10 images are usually selected to appear in the magazine. However, we like to have many more available to choose from. Some wide shots, some mid-range, and some close-up shots of individual features are desirable.

Images must be high resolution. The easiest way to determine this is to set your camera for the least number of photos, or the highest resolution, however that might be designated for the brand of camera you are using. Digital cameras capable of 6+ megapixel images should be adequate.

On-camera flash is not recommended, and the use of a tripod is strongly recommended. We look for photos that are evenly lighted, have sharp focus, and have good depth-of-field. They should have an easily distinguished primary subject. Do not crop your pictures! We will do that if necessary. If you are not doing the photography yourself, the photographer should receive credit. Sharp focus is important! We can

compensate for bad lighting using Photoshop, but we cannot improve poor focus. Please include a short description of each photo.

A track diagram is preferred for all layout features.

This diagram can be hand-drawn; our Draftsman will create the final product for publication. Overall dimensions of the layout, length and width, should be noted on the diagram. Any major layout features should be labeled, especially if they are referred to in the text.

A photo of the author/builder is also needed. This can be a photo of the author or builder with anyone else they may care to include (spouse, child, friend, pet, mentor, etc).

Author biographic information is needed for both layout and how-to features. This can include anything that the author cares to reveal about himself or herself; career, other hobbies or interests, club affiliations, civic activities, education, etc.

Captions for all the photos are needed. They can be written after the final images have been selected for publication. A thumbnail sheet of the selected images will be sent to the author so captions can be provided.

How-to articles should have a photo for every step in the process plus pictures of any special tools or jigs used. A picture is worth a thousand words.

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If you do not have the use of a computer, you may submit your story as a typewritten manuscript, single sided. Photographs should be glossy finish, a minimum size of 4" x 6".

SEND EVERYTHING TO:

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If you have any questions, don't hesitate to contact me at **dispatch@nasg.org**
Richard Caugherty, Editor

Turning those lemons into lemonade

Jack Halbleib, Chippewa Falls, WI, photod by the author

Having been a model railroader for over sixty years, I know the thrill of turning on the power and watching those trains go through their paces. But, just as in the real world of railroad operations, we sometimes encounter those disruptions and must take some down time to repair the problem.

If it is a simple matter or operator error (ouch, forgot to reset that switch on the main-line) or electrical glitch (hey, it was running simply fine last time I ran it), we all know the feeling.

Then there are the structural issues. Whether it is a snaking “s” crossover that we know should be changed for smoother operation (which I finally changed a couple of years ago) or, as I have put up with for several years, a significant loss of power in a long stretch buried under my town built on the second level of the layout. I tried all kinds of band-aid approaches, running cleaning cars, wiggling the track where I could reach it, adding jumpers.

Still my trains bogged down badly under my town, often stalling, so I had to unscrew my front access panel and reach into the maze of wiring used for lighting my buildings, track, and accessories to retrieve my stalled train. Finally, it was just high time to do it right and fix my track wiring for good.

It was not pretty. I had to re-



Look at all the buildings and street lights and imagine the massive amount of wiring underneath.

move my plywood base secured with wood screws. That meant taking down all my Plasticville and Flyer buildings, people, cars, trees, but leaving the wired lamps in place.

Oh, one other thing you should know, I use all Flyer terminals, original track and so

forth. You automatically know that results in poorer electrical connections than today's finer products would provide. Even my Plasticville buildings are not glued together, only press-fit. Add to that my hundred-plus people and cars and other accessories and you know that



Here is another view of my town that sits on top of my trackage and also the access panel with so many screws to undo.

just getting things ready to remove the plywood was important. (I am still looking for one side of the church belfry!!!)

The real focus of this short article is to remind my fellow S gaugers that you may as well take the opportunity to make a list of those other projects that could be incorporated into these “maintenance of way” efforts. I wound up running heavier wire to my block. The 690 terminal was rusted where it clamped onto the base rail. I cleaned and inspected the track and found that it was in decent shape. The layout was built almost 30 years ago, and I figure with all the Flyer smoke wafting inside the six-foot underground run, coupled with the heat from the miniature Christmas tree bulbs used to light up Plasticville (and the other thirty or so bulbs underneath I didn't need), plus being down the basement (with a dehumidifier running in the summer), it wasn't surprising to see some level of deterioration.

While I was “in town” for repairs, I wisely took the time to solder the wire connections to multiple Life-Like street-lamps which decades ago I had simply “twisted together” into a web of unreliable circuitry. How often as I reached under my town did, I bump a wire or two and had to jiggle and re-twist to get my town illuminated again! My 755 Talking Station had been silent for a couple of years (5?) from one of my forays into the wired jungle so, if I was “in town,” I got See Lemonade on page 26

Down by the Depot

by Carl Rudolph
Sacramento Valley American flyer Club



We spoke too soon last time. The Delta variant has reared its ugly head and our country is having a major COVID surge, mostly among the unvaccinated. But S gauge clubs are plowing ahead, in person or not, in the interests of maintaining personal and collective sanity. Here is the news that has come in:

The **Pioneer Valley Model Railroad Club** decided to have another Zoom meeting. **Steve Allen**, our correspondent, displayed **Tommy Robichaud's** partially completed boiler house (which has since been completed) Steve also displayed his own completed auto dealership which has nice detail and looks like a 1957 Ford Ranchero in the showroom! **John Robertson** reminded everyone that he has two S gauge layouts, with one in the garage. The club reminisced about one member who spent many a wintry night in that garage, but nothing further was mentioned. Your scribe is sure that there is a story about that lurking somewhere!

The **South Jersey S-Gaugers** are still meeting virtually also but the newsletter is full of good stuff. **Ed Claypoole** wrote a genuinely useful article on the history of Steamtown in Scranton (PA) and **Joe Sullivan** had a clinic report regarding a conversion of an old American Flyer 638 caboose to a "weathered" appearance and LED lighting via Ace trucks with American Models wheelsets. The travails of the conversion made for a fun read and the result was well done!

The **Baltimore Area American Flyer Club** have announced the first in-person meeting in a long time at **Jim McMorrow's** home with a backyard meet. Train shows are starting to resurface so the club was preparing to have the layout at the Great Model Train Show in Timonium if it takes place. The layout has had lots of improvements during the down time and the club is looking forward to its use and display.

The **Cuyahoga S Gauge Association** (Cleveland area) had another in-person meeting at the home of **Jim and Maryann Sima** at which the tinplate layout was hauled out and checked for problems due to the long-term storage caused by the pan-

demic. Sure enough, corrosion had bedeviled some electrical connectors so derusting and cleanup were performed to good advantage. On the sad and nostalgic side, **Bill Newhouse** and **Mike Udolph** have gone through the late **Lee McCarty's** train stuff and a date was set for selling all of it in 70 lots of parts and rolling stock to the club members.

The **Central Ohio S Gaugers** (Columbus area) were supposed to have the layout at a show in Dayton but discovered that the fairgrounds building to which they were assigned had no air conditioning. Ohio in mid-August and pandemic still ongoing and no AC? The mind boggles and the club wisely cancelled their participation. But the club still had the annual BBQ picnic to look forward to at the Marion Union Station which is a wonderful place to watch trains and peruse historical material. Anyone passing through the area is encouraged to make a stop at this location but be prepared to spend several hours! The club was also anticipating a trip to Georgia to pick up their brand-new trailer for layout transport. The trailer was financed by several worthy donors and the club is justly excited!

The **Central Ohio S Gaugers – Northern Division** (Toledo area) met at **Randy Bosscher's** home in Toledo but again no report yet. Meanwhile **John Frankforter** has been busy on his home layout building a sand and fuel facility at the engine servicing area. He was given a 3D printer so all the printed parts, including people, are purple because that is the color of the plastic that came with the printer! Once painted, though, they look wonderful, and John's work shows vast creativity and skill.

The **Southeast Michigan S Gaugers** met via Zoom again. **John Racey** is still working on the house model that he started under the New Tracks Modeling Program conducted by **Randy Bosscher** and **Gaylord Gill**, and it appears rather complete. **Bob Bubeck** made a trip to Strasburg (PA) to see and ride behind the Norfolk & Western 611 which was up out of Roanoke for an extended visit. Lastly, the club has scheduled its first in-person meeting in a long while at **Gaylord Gill's** home.

The **Badgerland S Gaugers** are still preparing to hold the Fall S Fest. **Ron Schlicht** sent out an email stating that the deadline for registration had been extended. By the time you get to read this it will be too late, so we hope you were able to go and enjoy yourself!

The **Pines & Prairies S Scale Workshop** (Twin Cities area) is busy and that is a fine thing about which to read. **Chris Poppe** is working on the layout and enticed the club over to help with Taco Salad for dinner. Sounds good to yours truly! Meanwhile **Shane Lambert**, who lives in the "Frozen Tundra" of Zimmerman, Minnesota (40 miles north of Minneapolis) has been feeding dinner to a few members of the club who then do a lot of work on his layout which is finally getting to the operations stage. Congratulations to Shane and the gang!

The Dallas-area **Lone Star Flyer S Gaugers** correspondent Larry Yurcho sent the following: "The Lone Star Flyer Club has continued to enjoy our return to in-person meetings over the past few months. Since all attending have been vaccinated, we enjoy the fun and fellowship of meeting and running our trains. We are working diligently to get our two new additions to the club's layout done before the October train show on the 2nd and 3rd. The new downtown area is not likely to be finished but looks like the new four bay roundhouse and turntable will be. This will provide a nice visual addition to our layout that will be on the side so visitors will have an unobstructed view of its character and functionality.

"After our regular monthly meeting on August the 14th, several members and some of their grandchildren braved the potential rainstorms and made their way to downtown Fort Worth to view the Big Boy engine as it made its tour through Texas. Those that saw it said it was very impressive. The visit also made most local TV news stations.

"All of our North Texas Council of Railroad Clubs are diligently preparing for the October show, despite an ominous increase (again) in Covid cases affecting our communities. We have planned several new additions to the show, featuring a new Great Train Race where attendees may race each other on a dual stacked set of figure 8 tracks! We will just be racing the engines, so no other consists to be concerned about.

See Depot on Page 26

Down by the Depot continued from page 25

The winner keeps racing until someone beats him or her. We expect this to be a highlight of the show. We are all keeping our fingers crossed that it would happen!"

In the San Francisco Bay area, the **Golden Gate American Flyer Club** finally scheduled an outdoor in-person meeting (and BBQ!) at **Al DiMatteo's** home, but it was very recent and no report on it yet. The Flyer Fest West which they host this year is coming together and you may have seen the notice elsewhere in this issue regarding the show and the Anchor Steam Beer commemorative cars they have for the show. Great looking cars! This promises to be a good show.

The **Southern California S Gaugers**

Club has held two (count 'em, two!) outdoor in-person meetings – one at the live steam railroad at the Orange County fairgrounds in Costa Mesa (their annual summer picnic) and another at a regional park near San Dimas. Both were well attended, and all had an enjoyable time according to the news. Train shows are still mostly on hold but there was the possibility of having the layout at the scheduled Great Train show in Victorville. The club has been discussing getting a new transport trailer for some time. So, **Steve Stallings** at the second meeting announced that he had seen a trailer on eBay that had the right dimensions and wheel placement, and was inexpensive, so went ahead on his own and

bought the trailer. We wish we could have been a fly on the wall to see the look on the members' faces! Then he hauled the trailer out and passed it around to the club! Perfect S scale with the club logo banner already installed! It sure instigated additional conversation about a real trailer!

It is hoped that more people will consider getting vaccinated now that the Delta variant seems to be appearing everywhere. The more control, the better the prospects for more in-person club meetings and train shows!

Keep trackin'!

**Email: clubcar11@comcast.net
or Carl Rudolph, 1940 Superior Drive, Twain Harte, CA 95383**

Turning those lemons into lemonade continued from page 24

the wiring fixed so that it the latest arrivals.

So, what is the point? Besides periodic maintenance we, like our 1:1 scale brothers, realize we are also subject to

more dire consequences. Like our prototypes, we can use the time to analyze the source of the problem, estimate the scope of the project, and then schedule the time and supplies to get the job done right.

Yep, there is a sense of satisfaction in doing the job right every time a train sails underneath my city, streetlights glowing, and stationmaster again finding his scratchy and mournful voice!!

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'Finger in the light socket' too often describes approach to model railroading

Jim Martin, Port Colborne, ON, Canada, Photos by the author unless otherwise noted

CamAm Convention Car for Christmas



As of this writing, Buffalo CamAm Convention organizers Darby Marriott and Ron Kemp say that next year's convention car will be available for Christmas shoppers.

The 70-ton three-bay American Models hopper, labelled for the Toronto Hamilton and Buffalo Railway, will be available in three versions...prototype number 1221... scale and hi-rail: and convention date 2022 in hi-rail only. The price per car will be 68 dollars US, which includes shipping to either the US or Canada.

Visit the NASG convention website to learn more of the history of this class of freight car as well as its extensive cross-border travels. The site notes that the iron ore pellets the cars carried were reddish brown in color. The pellets were quite small, so a fine grade of iron ore colored ballast might be the way to go when creating loads for these cars.

By the time you are reading this, ordering information should be posted on the convention website, nasg.org 'events,' '2022', or contact Ron directly at rgkemp316@gmail.com to place your order. Be sure to check the website often for additional convention information as it unfolds.

"Finger in the light socket" too often describes my approach to model railroading. Like the curious child who sticks his/her finger in the light socket to see what will happen, I occasionally will apply a new product or technique to my layout without proper judgement or caution. And every so often I am unpleasantly shocked at the results.

A couple of years back I produced a

custom mix of commercial ballast, cinders and builders' sand that I thought would represent the track ballast in the area I was modeling. The finished effect looked toy-like, but not before I had put down about 20 feet of the stuff in my Simcoe yard. Its appearance did not improve with age and bothered me every time the train room lights went on. Finally, this past summer I bit the bullet and removed it.

I like applying ballast. I find it Zen-like. I also enjoy the instant gratification of having my track immediately appear "real." However, removing glued-down ballast is another thing altogether. Luckily, I use di-



luted white glue which softens almost immediately when isopropyl alcohol is applied with an eyedropper. Then it is a simple but tedious matter of picking, brushing and vacuuming. The new ballast, sifted builders' sand, looks much better.

While I was redoing the ballast, it made sense to add the other final enhancements; detailing, painting and weathering. Why? Because track is a model too, just like structures and rolling stock.

Before re-ballasting, I added joint bars and rail braces to the rail sides, but only on



the outside edges visible to viewers. The joint bars are from Smoky Mountain and the rail braces come from the San Juan Car Shops On3 turnout detailing kit. Once they were glued in place with diluted Weldbond, I repainted the ties and rails with Raw Umber acrylic paint.

Then came the re-ballasting. After the glue had dried, I mixed a warm grey color of acrylic craft paint and dry-brushed the ties. The improvement was immediately apparent but also a little "punchy" in appearance. Weathering powders evened everything out. Grey weathering powder on the ties accentuated, every so often with

some light touches of dark yellow, brown or black gave subtle variations with which I was quite happy. Rusty brown powder applied to the rail sides finished the job.

This all sounds like work, but it took only a few evenings and for my small layout, was worth the effort. I hope after you check out the photos you will

think so too. These techniques will enhance any track from hand-laid to high-rail.

I am working with a greater sense of



purpose these days. I live only 30 miles from Buffalo, and although it is not listed on the Convention's layout tours, I plan to have my Dover Branch layout open to anyone who wishes to pop into Canada during that time. That invitation is providing me with the impetus to keep working away.

Till next time, Cheers, Eh?

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NEW MEMBERS

NASG New Members July 1, 2021 to August 31, 2021

Michael E. Butler.....Tucson, Arizona	Charles M. Hoffman.....Harrisburg, Pennsylvania
Gary J. Carmichael.....Imperial, Pennsylvania	James E. House.....Missoula, Montana
James E. Carroll.....Glasgow, Kentucky	Hampton H. Ingram.....Bassett, Virginia
James M. Cridlin.....Chiefland, Florida	Dan J. Jeffrey.....Gettysburg, Pennsylvania

William Moitz made a donation to the NASG

Deceased Memebers

Tim L. Horner.....Renova, Pennsylvania
Steven P. Salaway.....Winsted, Connecticut
John W. Stauffer.....Elkton, Maryland

DISPATCH DEADLINES CHANGED

for submitting time sensitive articles and advertising are December 15 for the March issue, February 15 for May, April 15 for the July issue, June 15 for September issue, August 15 for the November issue and October 15 for the January issue.



NASG on FACEBOOK

Chris Burger manages the NASG presence on **Facebook**. The link will take you to the NASG discussion group. You must have a Facebook account to be able view and participate. Go to **www.nasg.org** and click on **American Flyer** then click on **External Links** and then **Facebook** and then **NASG Facebook Discussion Group**

Calendar of Events

This page is for the listing of model railroad events, with emphasis given to S-gauge or S-scale. If you know of an upcoming event related to S scale model railroading or model railroading in general, please e-mail information to dispatch@nasg.org so that we are able to print it in a timely manner. **DUE TO THE COVID-19 PANDEMIC PLEASE BE AWARE THAT EVENTS LISTED HERE MAY BE CANCELLED OR RESCHEDULED. CHECK THE INDIVIDUAL WEB SITES FOR INFORMATION.**

2021 TRAIN SHOW UPDATES

A new corporation, called **GREAT AMERICAN TRAIN SHOWS, INC.**, is the parent company of these train shows: Greenberg's Train & Toy Show, Great Midwest Train Show, Great Train Shows and World's Greatest Hobby on Tour Shows. The Train Show web site www.TrainShow.com will remain the website for all shows.

November 5 through 7, 2021 — Milwaukee, Wisconsin FALL S FEST sponsored by the Badgerland S Gaugers will be held at the Four Points by Sheridan Hotel, 5311 S. Howell. Details at www.trainweb.org/bsg/org/events/sfest.htm

November 6 and 7, 2021 — Syracuse, New York 46th Great New York State MODEL TRAIN FAIR at the State Fairgrounds. 10 am to 5 pm Saturday, 10 am to 4 pm Sunday. The largest model train show in New York State! 300 vendor tables; 30+ operating layouts. Admission (16+): \$10; children (15 and under): Free. Sponsored by CNY Chapter, National Railway Historical Society. Information: Phil Edwards at 315-451-6551 or www.mod-eltrainfair.com

November 6, 2021 — Simi vaT1ey, California SWAP MEET AND OPEN HOUSE presented by the Santa Susana Railroad Historical Society. Santa Susana Park, 6503 Katherine Rd. Swap Meet 8 to 11 am, Open house 11 am to 4 pm. Entrance \$2 donation, kids under 12 free. The Southern California S Gaugers Modular Railroad is scheduled to be operating in the depot east room. All scales, equipment, scenery, books and photos. vendor Tables in park pavilion. For info e-mail santasusanapacific@gmail.com

November 6, 2021 — Taunton, Massachusetts NORTH SHORE MODEL RAILROAD CLUB FLEA MARKET at the Holiday Inn Taunton, 700 Myles Standish Blvd. Suggested donation \$3 adults, kids free. Newly acquired estate collections, thousands of items for sale, free parking. For more information call (781)-245-4742. www.nsmrc.org email: show@nsmrc.org.

November 13, 2021 — Louisville, Kentucky TRAIN SHOW & SALE by Division 8, NMRA Mid Central Region at the Holy Family Parish Safin Center, 3938 Poplar Level Road. 10 am to 3 pm. Adults \$6, Children 12 and under FREE. FREE Parking. Web site: <http://www.div8-mernmra.org/Dealer> Tables: Mark Hedge (812) 288-8037 (Voice) or div8trainshow@gmail.com

November 13, 2021 — St. Cloud, Minnesota GRANITE CITY TRAIN SHOW at the River's Edge Convention Center, 10 Fourth Ave. South. 9 am to 3 pm. B Wheelchair accessible. Admission: \$6. Kids 10 and under free Visit www.GraniteCityTrainShow.com

November 14, 2021 — East Lansing, Michigan LANSING MODEL RAILROAD CLUB SHOW and Sale at Michigan State University Pavilion, 4301 Farm Lane. Refreshments available, Wheel-chair accessible, Free parking. Admission: \$6 Per Person, under 12 free. Contact Ron St. Laurent at (517) 256-3588, or visit www.lmrc.org

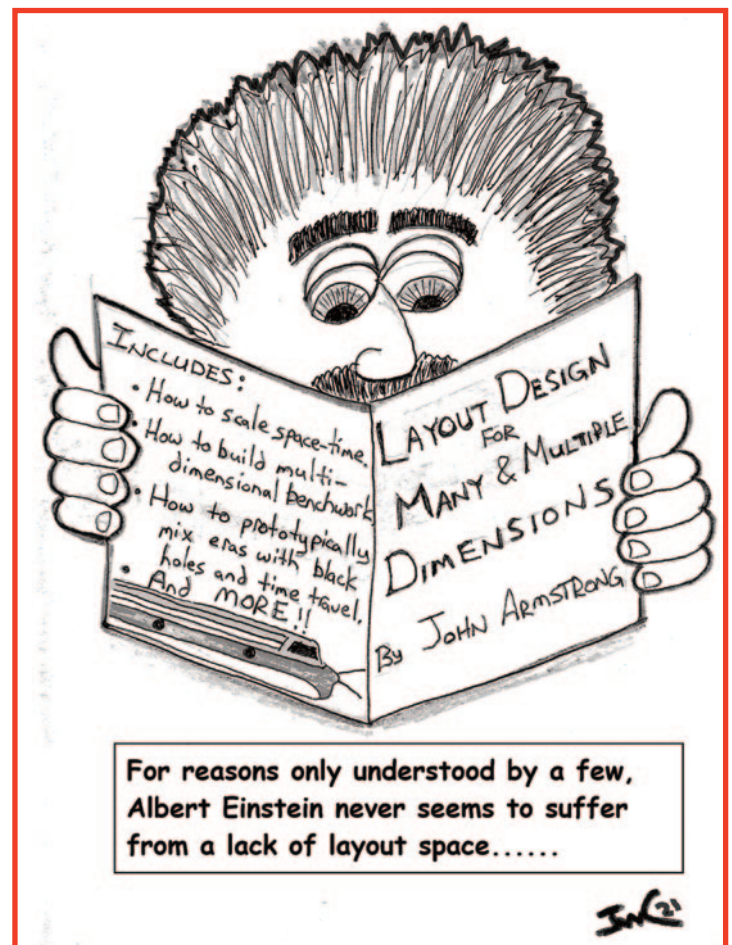
November 20, 2021 — East Greenville, Pennsylvania EAST GREENVILLE TRAIN SHOW at the East Greenville Fire Co, 4th & Washington Sts. from 9 AM to 3 PM, Hosted by North Penn S-Gaugers. All gauges, 4 oper-

ating layouts about 100 sales tables. Admission: \$3 per person, % for family, under 12 free w/adult. Free parking, refreshments. www.npsgauge.org for more inofrmation.

November 28, 2021 —Saline, Michigan RAILS TO WHEELS FLEA MARKET AND SHOW 10 am to 3 pm at the Washtenaw Farm Council Grounds, 5055 Ann Arbor Saline Rd. Adm: Regular \$4 per person, Children under 12 Free with adult. Early Bird \$10 per person (8-10 am) Parking FREE! Info: john Young 517-449-9063 or cdjhyoung@yahoo.com.

May 20 through 22, 2022 — Indianapolis, Indiana INDY JUNCTION 2022 TRAIN SHOW at the Indianapolis Marriott East, 7202 East 21st Street. Friday 5-9 pm, Saturday 10 am to 6 pm, Sunday 10 am to 3 pm. \$8 per person GOOD FOR ALL 3 DAYS! Children 13 and under FREE with paid adult Ticket. Sales to public onsite only. No advance ticket sales. Exhibitors: Small to medium sized manufacturers of model railroad products and supplies. Food concessions available. FREE parking and Much More! website: <https://www.indyjunction2022.org>

September 1 through 4, 2022 — Tacoma, Washington 42nd NATIONAL NARROW GAUGE CONVENTION at the Murano Hotel, 1320 Broadway, just 22 miles south of the Seattle/Tacoma International Airport. Go to www.seatlengc.com



CLASSIFIED ADS

ANY ACTIVE MEMBER of NASG can place a non-commercial classified ad in the Dispatch for no charge. Ads must be limited to 10 lines+. E-mail your ad copy to dispatch@nasg.org or mail to Richard Caugherty, Editor, 714 Twin Oak Drive, Pittsburgh, PA 15235. Please be sure to include your member number. Ads will run for three consecutive issues unless told to stop.

FOR SALE

FOR SALE: Downsizing, selling much of collection of S gauge trains. Send email to norm.curtiss@att.net for my Four Sale List.
FOR SALE: Pre-war American Flyer Hudson engine, tender, 5 cars. Fair to poor condition. Pictures available. Make offer. Contact: Roger Sebenik rsebenik@comcast.net.

FR SALE: Af #49614 PRR passenger set includes 2 Alco PA-1s & four cars, add-on dining car and full length domw car, C-10/P-10, free shipping, \$500. AF #355 C&NW Baldwin diesel, E, \$65, #21801 C&NW Baldwin diesel, E, \$75, #21085 C&NW 4-6-2

steam, E, \$80. Call between 9AM & 9:30 PM EST 412-756-7789

FOR SALE: Large collection of 3-rail O-gauge locomotives, rolling stock, track and accessories. Lionel, MTH, Williams, Weaver, Marx and others. E-mail rrbill39@gmail.com for a pdf with pictures and prices or send an SAE to **Bill Pyper, 4406 21st Court SE, Lacey, WA 98503 for hard copy.**

FOR SALE: AF #15 Directonic Rectifier \$20, AF NTTM Work Train: #6-48240, #6-48230, #6-48234, #6-48247, #6-48226, #6-48223. Total all \$199, plus shipping. Your address needed to calculate shipping. Rob-

ert E. Hartzell, Phone **610-799-2470**

FOR SALE: Reducing my collection/accumulation at age 75. Included scale, AF and Bachmann On30. Priced to sell. Email: gmheugene@gmail.com for an updated list. Mostly freight cars, but the first engine offered is American Models GP-35 decorated for National Railways of Mexico.

WANTED

WANTED: S Gaugian magazine July/August 1977 and/or September/October 1977. Call between 9AM & 9:30PM EST 412-759-7789

WANTED: Classic Trains/American S Gauge switches and track. Fritz Raab fraab@gmrr.biz, 515-275-2141

The 2021 NASG Annual Member Car

We are happy to announce that our 2021 annual car for NASG members only will be a PS-2CD 4427 3-bay Covered Hopper in a GE Railcar Services /TLDX scheme called "Gold Kist – Atlanta, Georgia."

We have again contracted American Models to provide another high-quality model. This will be in the original gold-colored sides and black ends. It will be available for \$68 inclusive of shipping in

two road numbers and in either scale or high rail/AF compatible versions.

We are expecting delivery in July.



To order: go to the NASG Store at www.nasg.org/store and go to the Annual Member Car page.

2020 Annual Member Car

We are down to a small remaining inventory of the 2020 MNS 50' boxcar. To order: go to the NASG Store at www.nasg.org/store and go to the Annual Car page.



CLUB CARS FOR SALE

ANY MEMBER CLUB with special edition cars from conventions or fests can advertise them here for no charge. Send a hi-res jpeg photo and information to dispatch@nasg.org

North Florida Div. of Atlantic Coast S Gaugers is offering Lionel AF boxcar in Florida's colors to support North Florida ACSG. \$100 SH included or \$80 @ a train show. Check to Larry Shughart, 5216 Lourcy Road, Jacksonville, FL 32257



2019 NASG CONVENTION BOX CAR featured the US Playing Card Co. Bicycle logo. The white bodies/blue roofs display the red logo #1985 & red roof displays the blue logo #2019. Hi-rail & scale available for \$70 plus shipping. Visit www.COSG.Club for info and order form or call COSG 614-766-9033



S FEST 2018 ILLINOIS CENTRAL REEFERS CASG offers yellow and orange reefers for \$57 yellow & orange (only 32 produced) each in 2 different road #. Shipping \$7.95 per car. Specify hi-rail or scale. Contact Leigh Maginniss 847-401-8672 for availability. To order specify cars desired, mail check payable to CASG, to Leigh Maginniss, 314 S. Drury Lane, Arlington Heights, IL 60004

SOHIO TANK CAR These Lionel American Flyer cars sell for \$70 including shipping. Make checks payable to SCSGC, mail to Jeff Kruger, 1865 Nottingham Lane, San Dimas, CA 91773-3718



BURLINGTON REEFERS State-Line S-Gaugers have reefers for \$65 including shipping. To order make checks payable to SLSC, mail to Tom Behles, 15804 O'Brien Rd., Harvard, IL 60033



2017 CONVENTION CARS AND TRAILORS TOFC \$60 each, TRAILERS \$20 each



Shipping: \$11 1st item, \$4 each additional item. **BALTIMORE AREA AF CLUB** has TOFC & trailers B&O #2017 in scale, B&O #9523 in hi-rail & scale, WM #1889 & #2411 in scale & hi-rail. Extra trailers available. <http://baltimoreamericanflyerclub.org/wp-content/uploads/2017-CarForm5R-1.pdf> Order from BAAFC, c/o Kyle Russell, 4146 U Way, Harve de Grace, MD 21078



Golden Gate American Flyer Club has a Deep Rock tank 2 car set for \$159.95 + \$15 shipping. Checks payable to GGAFC, send to Paul Guaraglia, P.O. Box 4054, Foster City, CA 94404-



2012 S FEST SOO LINE CONDOLAS in limited supply with loads @ \$40 ea. + shipping \$10 first car, each add. \$4. Loads avail. in 3 colors Hi-rail, or Scale. #2012 or 67491 **2016 S FEST SPRECHER REEFERS** limited supply \$55 for Hi-rail, \$60 for Scale. Shipping \$10 first car and Each add. \$4. # 2016 or 20025. Call 414-421-8512 or rons48tblue@gmail.com



COMPANY STORE

FOR SALE TO THE GENERAL PUBLIC

NMRA S-3 TRACK AND WHEEL GAUGE
\$7 members

NASG S-3, S-4 TRACK & WHEEL GAUGE \$7 and 2 for \$12

REFRIGERATOR MAGNET
50TH ANNIVERSARY LOGO \$4 each

#802 and #5 KADEE COUPLER HEIGHT GAUGE Measures car on track proper 17/32" height. \$6 each

S SCALE CLEARANCE GAUGE
\$7 members
\$9 non-members

NASG CAP
\$19
Tan & blue denim with color logo

NASG GOLF SHIRT
\$29
2X \$33 dark blue w/pocket yellow logo

CLASSIC ERA

MODERN ERA

EMBROIDERED PATCH
\$4
2 3/8" diameter

FOR SALE TO NASG MEMBERS ONLY

NASG T-SHIRT
\$15
2X \$18
ash gray with blue logo

LAPEL PIN
3/4 diameter
\$5

NATIONAL ASSOCIATION S-GAUGERS

TO ORDER any of these products, download an order form at www.nasg.org/Store or write your order legibly on a piece of paper. Make checks payable to NASG Company Store and mail to: **Shane Lambert, 2091 Vining Drive, Unit D, Woodbury, MN 55125**

IMPORTANT: Be sure to include your member number, quantity desired and sizes. **PRICES include shipping to anywhere in the continental US.** All shipping is by US Mail. For shipping outside the US contact the Company Store. **Wisconsin residents** please include appropriate sales tax and county of residence. If you have questions contact Storekeeper Shane Lambert 651-689-3206 or e-mail to companystore@nasg.org
TO SHOP ONLINE: Go to <http://www.nasg.org/Store/CompanyStore.php>
Choose the items that you want to purchase and click Add to Cart and follow the instructions.

2019 American Flyer Commemorative Car \$69.00

includes domestic shipping. Since 1981 NASG has contracted with Lionel to produce a limited-run car so a caboose represents the end of the line. **NOT PART OF SALE**



NASG CAR DISCOUNT SALE

ALL CARS ON THIS PAGE EXCEPT THE 2019 CABOOSE ARE PRICED AS FOLLOWS
Buy five or more cars — Take 30% off
Buy four cars — Take 25% off
Buy three cars — Take 20% off
Buy two cars — Take 15% off
Buy one car — Take 10% off

Please check the NASG website for the most up to date information on car availability since some may sell out by the time The Dispatch is printed.



2017 and 2018 American Flyer Commemorative Cars \$69.00 each or two for \$130.00



2016 American Flyer Commemorative Car \$79.00

A CNW flatcar loaded with pallets. Extra pallets 4 for \$10.00.



2015 Convention Car \$68.00 hi-rail only

Kansas City Southern PS2 hopper car by MTH
Numbers 286707 or 286815

2015 AF Commemorative Car \$69.00 Lionel BNSF waffle sided boxcar



2010 Jenney Gasoline Triple Dome Tank Car
\$75.00 by Lionel road number 2910



2006 General Electric Crane Car \$70.00
2007 General Electric Boom Car \$52.00
BOTH CARS ONLY \$115.00

TO ORDER THESE CARS

Make payment out to NASG and mail to:

Clay Buckage, American Flyer Commemorative Car Project,
685 Barlett Court, Brentwood, CA 94513-6205
925-679-4887

Please include the following in your correspondence:

1. Your NASG ID number from your membership card.
2. Your name and shipping address.
3. Which car or cars that you want and how many of each.

4. Your telephone number or e-mail address in case there are any questions about your order.

All prices include shipping to addresses in the United States. Canadian orders please add \$10. To ship outside the continental US or for any other question contact Clay Buckage at clay@da-vestnt.com

TO SHOP ONLINE Go to www.nasg.org/Store/AFCars.php for the AF cars or www.nasg.org/Store/ConvCars.php for Convention Cars and click on the **Add to Cart** button for the item(s) that you want to purchase and follow the instructions.

New items 2020

www.americanmodels.com



Milwaukee Road GP9



Milwaukee Road S12



Lehigh Valley S12



Lehigh Valley RS3



Burlington Northern Santa Fe.. GP-35



CSX GP-35

Available in 3 numbers



Toledo Peoria & Western
3 bay coal Hopper.

Available in 3 numbers



Toledo Peoria & Western
40' AAR Box car



USRA Pacific 4-6-2 die cast locomotive with die cast Vanderbilt tender.

Coming Soon: Our new Pacific with new USRA cab.
Order now for scale, DC HR or AC HR engines with or without sound.
All engines come with smoke and choo choo sound.
We will print any road name on tender and cab. DC versions \$499.95.

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