

THE

# DISPATCH

MARCH-APRIL 2021



**COVER:** The cover photo this month is rather unusual in that it is a working 3-D printed S scale model of a CNR, class N-3 Consolidation. Simon Parent, member of the Canadian based S Scale Workshop, has been experimenting with 3-d printing for some time. Please see Jim Martin's article on page 18 for details.

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## NASG DISPATCH

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Joe Kimber, Ed Kirstatter, Jim Martin,  
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### SUBSCRIPTION RATE

1-year, \$30<sup>00</sup> includes membership in the NASG. Subscriptions payable in US funds. Postage paid at Pismo Beach, CA. DISPATCH is printed in the USA and mailed in the issue month.

### MEMBERSHIP YEAR

The Membership Year is determined by the date that one enrolls, guaranteeing 6 issues, starting with the next issue. All applications, renewals and membership questions should be directed to:

### NASG Membership Secretary

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P. O. Box 268, Middlebury, CT 06762  
e-mail: [nasgmem@comcast.net](mailto:nasgmem@comcast.net)

### ADDRESS CHANGES

Must be reported to Dick Kramer at the addresses above and received 30 days prior to publication of the Dispatch. No replacement issues sent after that.

**DEADLINES** for time sensitive articles and advertising are January 1st for the March issue, March 1st for May, May 1st for the July issue, July 1st for September, September 1st for the November issue and November 1st for the January issue.

**NOTICE TO READERS:** All warranties and claims made by advertisers in the DISPATCH are the sole responsibility of the advertiser. The NASG reserves the right to refuse any advertising material for any reason. The selection and/or use of the products or services and the application or results obtained from these advertisers is the sole responsibility.

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# Down by the Depot

by Carl Rudolph

Sacramento Valley American flyer Club

As this column is written, the first COVID vaccines have been administered. There is hope that a large majority of the population will get vaccinated and we can put this disease behind us (for the most part). Then we can start having real club meetings and quit being afraid of each other, which would be a big boon to all of us. But as usual the club members are inventive and find ways to keep sane until that day! Here is what we have heard:

The **Pioneer Valley Model Railroad Club** now has more zoom meetings under its belt, and they are still being successful. Steve Allen reported that Look Park tried to do something virtual with Santa's trains, but it sounds like they just could not pull it off – so the club did not have to figure out how to man the train display this year. **Gene Maag, John Robertson, Bill Morris** and **Steve Murphy** all showed current projects to the virtual crowd. All these people have great talent and creativity.

The **South Jersey S-Gaugers** have continued to hold zoom meetings too. While attendance may be down a bit compared to in-person meetings, the virtual meetings are enjoyed by those who attend. **Hank Worrell, Tom Burns, Michael McConnell** and **Ken Palmer** were able to create the annual static display at the West Deptford Library of various S gauge trains, all of which are beautifully displayed with interpretive notes on panels – looks like a museum display! Nice.

The **Baltimore Area American Flyer Club** members are getting cabin fever like everybody else so there was a large response of people willing to help set up the layout at the B&O museum, which, as far as we know, was not cancelled due to pandemic restrictions. The show at the museum runs for nearly the first half of December so a lot of folks, even with social distancing, should have seen the club's layout.

The **North Penn S Gaugers** have been able to meet in the community clubhouse under the current rules of restriction. They have discussed which future shows in which they would like to participate and are planning on setting up the layout at the clubhouse for members to operate. **Carl**

**Goldberg** donated a Union Pacific Northern freight set to the club, so we are sure there is interest in running that!

The **Cuyahoga S Gauge Association** (Cleveland area) have gone back to not having meetings due to the upswing in Covid cases. They are realizing that it will have been two years since either of the club layouts were last used before there is a venue for them again so probably there will be an all-hands-hit-the-deck effort to get things shipshape again before the curtain goes up! The newsletters are filled out with great articles of history in huge articulated steam locomotives, the end of the caboose, and the development of Westinghouse's air brake system.

The **Miami Valley S Gaugers** (Dayton area) are still having well-attended zoom meetings. The newsletter had a nice article by member Joe Leech who started from no knowledge at all to having a scale layout, with scenery, in a room that previously had been the pool room (had to sell the pool table!) and the numerous photos show his work to be quite good. Shows that you do not have to have decades of experience to make a nice layout! And **Tom Hartrum** now has some masked people waiting for the train at the wayside shack; he says when the pandemic is over, they will become bank robbers as soon as he gets the bank built! Nothing like a little humor right now!

The **Central Ohio S Gaugers – Northern Division** have had no new meetings but keep track of each other through email. **Tony Palumbo** thought it might be a good idea if everybody shared what was currently on the train workbench. Accordingly, several members followed through with plenty of photos and explanations. A few of **Randy Krajewski's** projects are shown in nearby pictures, all of which are made from American Flyer parts with a few added details. All very clever. **John Frankforter** is working on a couple of bridges, one curved, and Tony Palumbo showed a Plymouth "critter" he made to solve a switching problem at a local industry. More creativity on the loose!

The **Southeastern Michigan S Gaugers** have started sending newsletters

again which we certainly appreciate. Lots of activity here too with zoom meetings and reports from several members. Bob Bubeck reported on a new AF Berkshire locomotive and on a trip to the Age of Steam roundhouse; Raymond Hall, who apparently lives in Ireland, reported on the Railway Preservation Society of Ireland; Mark Charles spoke about locomotive restoration projects; and a whole bunch of guys reported on current projects for the Show and Tell portion of the programs. Highly active club which is good to see.

The **Stateline S Gaugers** started sending newsletters again (thanks!). All meetings since last Spring have been zoom virtual meetings. They had been preparing to host the Fall S Fest in 2021 but the Badgerland club, hosts for 2020's Fest, cancelled the event due to the pandemic and they will plan on hosting in 2021. So, the Stateline club will host in 2022 and plans have been put on the back burner accordingly. Like all other clubs (as far as we can tell) the Christmas/holiday dinner/party was cancelled too.

**See Depot on page 4**



Elevated yard shanty. Photos by Randy Krajewski.

# Depot

Continued from page 3

The Dallas-area **Lone Star Flyers S Gaugers** club also continue to hold zoom meetings with about ten members regularly “tuning in” for the meeting. Discussions are held about what progress is being made on home layouts, new purchases, and railroad memorabilia. The North Texas Council was still scheduling their winter show (as of the date of the note from **Larry Yurcho**) but the club voted to pass on displaying the layout there. The layout, by the way, is nearing completion of the “downtown area” which features moving sidewalks! **Scott Gore** oversees this effort with **Joe Borgert**, **Bryant Hindman** and Larry helping.

We have heard from a club new to the column! The **Gadsden-Pacific Toy Train Museum** in Tucson, Arizona sent in some news about who they are and their activities. The club has about 120 members who have indoor layouts from G to Z scales. There are larger scale layouts outside on the museum grounds. The S gauge layout is 11’ by 17’ with five loops for continuous operation plus several yard tracks; most rolling stock and locomotives are American Flyer. Of course, the museum has been closed to the public but the members are busy improving the layout with new additions, repairs and new landscaping. When the all clear is sounded, the club will return to its normal schedule of open houses twice a month at the museum.

In the San Francisco Bay area, the **Golden Gate American Flyer Club** had tentatively planned a meeting at a member’s home because it appeared the pandemic was easing there but then case loads surged again and the meeting was change to zoom type once more. No news about what transpired during the meeting as of this writing.

The **Southern California S Gaugers Club** met by zoom too. New member **Ron Wells** showed a modified AF searchlight car on which he removed the searchlight



An oil and sand storage facility.



An oil transfer facility.



A milk can transfer facility.

and added a Lionel yard light for MOW use in night work. He added a fuel tank and pipe to the generator. No photo sent but your scribe thinks it must be cool. In other business, the club discussed the merits of having a trailer or a storage unit for the layout modules and the knotty problem of where to put the trailer or where the unit might be located. Here in the West, we S gaugers are a lot more scattered geographically so it is more critical to find a central

location than in the east where distances are not so great. We will see how they progress on this project.

Still quite a few clubs missing from communication but that is understandable under the circumstances. We are all really tired of this mode of living but there is a light at the end of the tunnel, and it is not a train coming! Keep trackin’!

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All photos by  
Randy  
Krajewski



## President's Message

Will Holt, NASG President

### OUR NATIONAL CONVENTION

As of this writing, the CanAm Buffalo Convention is moving forward. There remains a possibility of its postponement. Please watch for any announcements in the next couple of months.

Since we had to cancel the 2020 convention in the Twin Cities, this would be a great opportunity for many, many members of our association to travel to the Buffalo, New York area to meet with fellow S scale 1:64 model railroaders.

We need to have a chance to sit across the table from our friends, make new friends, and enjoy our hobby together again once more.

### CHANGES FOR THE NASG

As many of you are aware, there have been several projects underway to bring S scale and the NASG into the 21st Century. Many of them are now complete.

As a follow up to the complete revision of our constitution, the Bylaws needed to be reviewed and adjusted. That project has been completed. By the time you read this, the revised and updated Bylaws will be approved by the required 2/3 majority vote of the Board of Trustees. The complete current version is posted on the web site. If you do not have access to the Internet, please contact the secretary to obtain a copy if you wish to have one.

Since its founding, the NASG has been classified by the Internal Revenue Service as a Non-Profit 501c (7) Fraternal Organization. While in the early years we were largely a fraternal group, over the years we have evolved into an organization that is much more devoted to education. We educate the public about S model railroading, the *Dispatch* regularly publishes articles that are educational, and our conventions have clinics and tours that are educational. With all of that in mind, we applied for a designation as a Not-for-Profit 501c (3) Education Organization and are awaiting a response from the IRS. If approved, this will make contributions to the NASG tax-deductible, and make the organization eligible for funding from foundations for appropriate projects.

After having been in place since the late 1980's without any significant review, the standards for modular/portable layouts are being updated and are nearing a complete revision. To put that into perspective, the standards pre-dated the advent of command control systems, so wiring requirements needed extensive changes. The revisions brought about a change of format to have specific standards for critical aspects of portable/modular layout building and added recommended practices for flexibility in design.

The convention manual is nearing a complete revision. It will be shorter, more concise, and be of great help to any S scale model railroading organization preparing a proposal to host a convention, planning and staffing a convention, and assuring a quality and profitable event. The manual may be downloaded from the web site or a copy can be provided by the Chairman of the Convention Committee.



## New Orders

Richard Caugherty, Editor

Well, it is almost one year since I became editor of this magazine. Time flies by before you know it. I hope this finds you all well and getting ready to enjoy some warmer weather soon?

My knee has healed nicely but requires me to maintain my exercise program at least until my operation anniversary. I have exhausted my physical therapy sessions according to my insurance. Enough about me.

### ANNIVERSARY IS UPON US

Since it is coming on to my anniversary, I want to take some time and thank people who without their help I would not have been able to get a magazine out to you. First up is Bill Pyper who gave me all the necessary information on how he had been publishing *The Dispatch*. His guidance was invaluable. You cannot imagine the information he was able to provide and walk me through all the steps one by one. So, thanks are the only payment I can send to you. Keep feeding me information and articles.

President Will Holt for continuously checking to see

how I am doing and what do I need. He has also been a great sounding board for ideas I want to use. He also provides information and articles for the magazine.

Next thank you to all the members who have submitted articles and pictures. Without you there would be no *Dispatch*. If you have been thinking about writing something, please do. Editing is my job, so do not worry about your skill set. All I ask is that everyone use the Guidelines as printed on page 7 of the January-February 2021 issue.

### IN THE FUTURE

I have asked before for articles and pictures of members' layouts and we have had some genuinely nice ones submitted. Hoping more of you will be willing to share your layout with all of us. Even if not completely finished, we are still looking.

Lastly, I would like to have a backlog of articles for when I don't get enough submitted for a certain issue. I have used most of what Bill Pyper had forwarded to me from his backlog. Editor's always worry about not having enough copy. Hopefully, you will come through so I do not have to cut the number of pages in a future issue. Looking forward to those emails.

## NASG OFFICERS

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### LIBRARIAN: Dale Minard

### AMERICAN FLYER COMMEMORATIVE CAR

COMMITTEE CHAIR: Clay Buckage

### MEMBERSHIP OUTREACH COMMITTEE

CHAIR: Ken Zieska

CO-CHAIR: Rob Whipple

## Member Dues

As of April 1, 2016, the yearly member dues are as follows:

\$18<sup>00</sup> One-time 6-month Trial Membership

\$30<sup>00</sup> Regular

\$50<sup>00</sup> Contributing

\$40<sup>00</sup> Family

\$50<sup>00</sup> Club, Business, etc.

You can now **renew your membership online**. Go to the web site **www.nasg.org** and click on **Membership**. Click on **renew online**. Enter your name and member number and follow the instructions. Have your credit card ready. You can still renew your membership by sending a check or money order by mail, payable to NASG, to:

Dick Kramer, Membership Secretary,

P. O. Box 268, Middlebury, CT 06762-0268

## NEW MEMBERS

### NASG New Members November 1, to December 31, 2020

John M. Brennan.....Buffalo, New York  
Michael Broderick.....Estes Park, Colorado  
Dr. William L. Clark.....Granbury, Texas  
Andre R. Garneau.....Leominster, Massachusetts  
Alan W. Jefferson.....Baltimore, Maryland  
James Kalasinski.....Amsterdam, New York  
Bill H. Keeling.....Chino Hills, California  
Kevin L. Kizer.....Three Rivers, Michigan  
Harvey F. Krueger...North Hollywood, California

Wilbur McCasky.....Crescent City, Illinois  
Clayton W. Moore....Carmichael, California  
Glen Morreale.....Inwood, West Virginia  
Brian Rogers.....Mystic, Connecticut  
James L. Row.....Steilacoom, Washington  
Jasmes Scarsella.....Farmington, NY  
Robert D. Shepherd.....Rio Vista, California  
Gary R. Tate.....Cedar Hill, Texas  
Bruce A. Vakiener.....Easton, PA

### Deceased Members

Robert B. Nicholson.....Fort Madison, Iowa

## DISPATCH DEADLINES CHANGED

for submitting time sensitive articles and advertising are December 15 \* for the March issue, February 15 for May, April 15 for the July issue, June 15 for September issue, August 15 for the November issue and October 15 for the January issue.



## NASG on FACEBOOK

Chris Burger manages the NASG presence on Facebook. The link will take you to the NASG discussion group. You must have a Facebook account to be able view and participate. Go to **www.nasg.org** and click on **American Flyer** then click on **External Links** and then **Facebook** and then **NASG Facebook Discussion Group**.

**COMMITTEE UPDATES AND CHANGES  
NATIONAL ASSOCIATION OF S GAUGERS, INCORPORATED  
DECEMBER 2020**

Over the past few months some changes in various NASG committees have become necessary.  
These changes include:

**AMERICAN FLYER COMMEMORATIVE CAR COMMITTEE:**

1. Dave Blum Resigned
2. Clay Buckage Has Accepted the Position of Coordinator  
Effective Immediately  
(Transfer of the Remaining Inventory Is Underway and  
Resumption of Sales Is Expected to Resume Around March 1, 2021.  
The Inventory Liquidation Sale Will Resume at That Time)

**CONVENTION COMMITTEE:**

1. Dave Blum Resigned
2. John Myers Has Accepted the Chairmanship of the Committee  
Effective January 1, 2021

**RUSSELL M. MOBLEY MEMORIAL LIBRARY:**

1. Michael Greene Resigned
2. A Librarian Is Being Sought
3. Disposition and Transfer of the Library Properties Is Being Evaluated

**NEW AD HOC COMMITTEE:**

Membership Outreach Committee

1. Ken Zieska Has Accepted the Position of Chairman
2. Rob Whipple Has Accepted the Position of Co-Chairman
3. The Purpose of the Committee Is to Study Ways  
To Recruit into S Model Railroading:
  - I. Teens
  - II. Young Adults
  - III. Young Marrieds
  - IV. Women
4. Committee Report Due by July 1, 2021

**The association also needs volunteers for several other positions including:**

**Assistant Treasurer:**

Experience with bookkeeping, Quick Books, Invoicing and Collections

**Convention Committee:**

Members with Experience in Planning Events and Meetings

**Promotions Committee:**

Several Programs Need Personnel for Implementation

**Service Committee:**

Chairman Needed  
Constitutionally Mandated Standing Committee

# NOMINATIONS FOR THE ANNUAL AWARDS OF THE NATIONAL ASSOCIATION OF S GAUGERS

For the first time, the Board of Trustees is soliciting nominations from the membership for any of the awards presented by the association (in the past, nominations were accepted but not solicited).

Each nomination must include the name of the nominee for the award and include a brief presentation (no more than 50 words) of why the board should consider presenting the award to that nominee, along with the name of the member in good standing making the nomination.

Except for the Bernie Thomas award, any member may nominate himself or his company for consideration. Nominations for the Trustee's Award are not accepted.

The nomination period is from March 1 of the current year until April 15 of the current year. Nominations should be sent to the president either via e-mail or in writing via post.

The Bernie Thomas award is pre-

resented each year to an individual whose range of activities have been of meritorious service to S, 1:64 model railroading and/or have been of meritorious service to the National Association of S Gaugers.

The nominee's activities should be of significant benefit to S over a period of years and over a wide range of geographic areas. It is conferred upon only one individual each year and is the highest honor the association may bestow. It carries with it enrollment as an honorary life member of the N A S G. This is the only award that the association must make annually.

The Charles Sandersfeld Memorial Manufacturer's Award is presented to the company releasing the most significant new product for S during the association's fiscal year, July 1 through June 30, or for a yet unrecognized manufacturer whose ongoing contributions to S have been significant.

It is awarded to the product and the company, not to an individual. It need not be presented annually.

The Perles Publication Award is presented to an author for an article that promotes and displays S model railroading in a publication that is for the broad model railroading community, not exclusively S.

The article must appear in a publication between April 1 of one year and March 31 of the following year (this provides for time for the article to be distributed to the Board of Trustees for consideration).

Especially important is that the article provide strong references to S model railroading. It need not be presented annually.

The Josh Seltzer Web Site Award is presented to one private individual or S organization for a new web site that promotes S model railroading. No web site may receive the award more than once. It need not be presented annually.

## Minard named to become NASG librarian

The NASG is pleased to announce the appointment of Dale Minard as the librarian for the NASG Library.

Dale became a member of the NASG in 2011. He holds membership in the San Diego S Gaugers, currently serving as librarian. With the resources he has available to him, he can probably handle many requests the library might receive as of now.

As for the library, arrangements are being made to retrieve the library properties from Michael Greene. This will probably be

sometime in late March or April. The properties will be brought to the Chicago area for temporary storage. As some of you may know, NASG member Dave Jasper is close to completing his longtime dream of creating a museum. As part of the complex, Dave will have a library and has graciously agreed that the NASG library can be housed there. The final transfer of the library properties will probably be late Spring. Ted Larson has agreed to assist Dale with the digitizing of the library properties.

Dale volunteering as Librarian, the digitizing of the library and it finding a home where the public can visit and utilize will be an asset to the NASG.



**Dave Blum**  
**PIKESVILLE MODELS**  
3315 Timberfield Lane  
Pikesville, MD 21208-4425  
410-653-2440

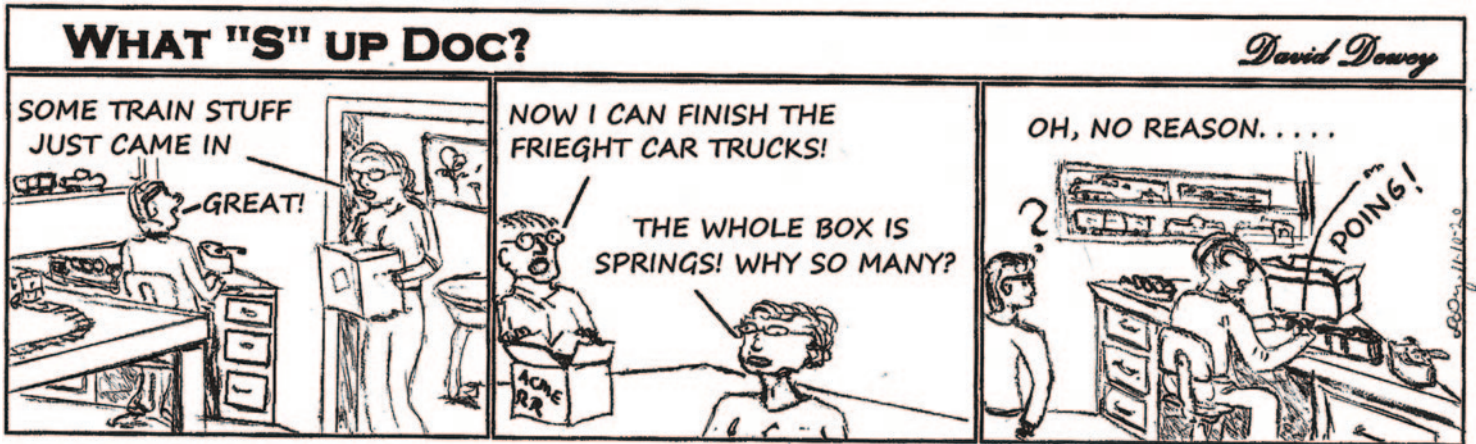


**ALL THE FLYER YOU DESIRE!**  
E-mail: [americanflyertrains@gmail.com](mailto:americanflyertrains@gmail.com)



# Something new!

The Dispatch advertised for a cartoonist to brighten our black and white pages. Below are printed submissions from our members. Please send us your thoughts.



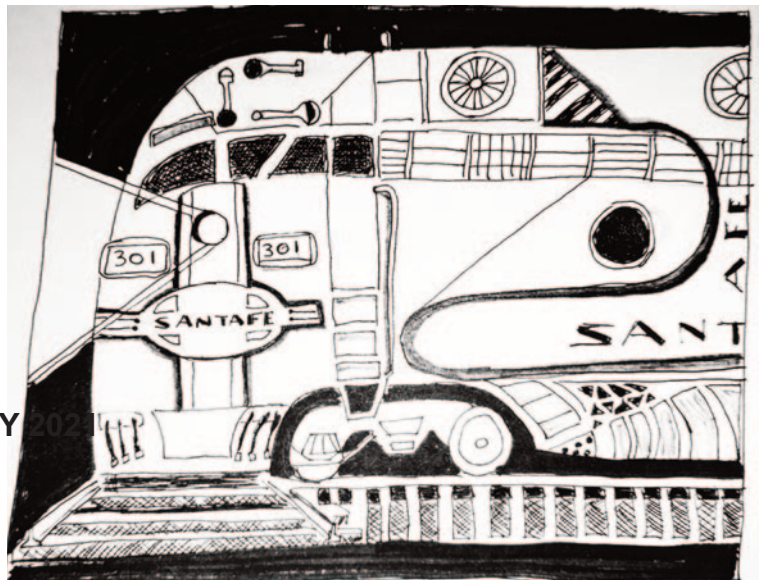
The strip above is by David Dewey. The two cartoons blow and left are from Joe Coen. These are samples and not necessarily how the finished product will appear.



8 NASG DISPATCH, JANUARY-FEBRUARY 2021

The club will always remember when former NBA center "Stretch" Sticklemeyer invited the gang to operate his eye-level layout....

Joe



If Pablo Picasso worked for EMD....

Joe

# LED *lighting from a big*

**Jerry Poniatowski, Wayne, MI**  
photos by the author

I never considered myself to be a lighting expert, but I will be glad to share what I did on my full-length Osgood Bradley New Haven “American Flyer” cars, which were made following an excellent article by Dick Karnes.

First, the LEDs themselves are from Home Depot and are a strip of under the counter lights that are powered by a small “wall wart” transformer that puts out 12VDC. I have heard of guys getting this kind of thing online, but I did not want to wait, and pay shipping. Most big box stores have these lights as well as do various online sources.

The strip has markings where you can cut a part of the end in 3 LED groups without affecting the circuitry. Each group of 3 then can be powered by another 12VDC source. In my cars, I used three 3-LED sections totaling a little less than 12” in length. So, each 3 LED section is a bit less than 4”, which would be perfect for a caboose, for example.

In my case, with absolutely NO room for onboard batteries, I opted for track power for the lighting. I suppose the battery boxes of a passenger car can be made to house actual batteries, but that was not going to work in my application.

The first problem to overcome is reducing the voltage to make them dimmer since a full 12VDC is WAY too bright. The built-in circuits of the strip can take care of up to 12VDC, but I may run the cars in a train on more voltage than that and on AC. I was going to simply install a proper resistor in the circuit to control brightness and be done with it. But to run on either AC or DC would mean a silicon full wave bridge rectifier is needed. Then a small capacitor to minimize flicker, and some sort of regulator.

In my application I am extremely lim-

ited in space for components. I found out that there is such a thing as a “buck booster module” which would allow the output of the module to be constant no matter the input voltage. Great! But they are way too large. Someone suggested I could put the circuitry in the vestibule. OK for some applications, but since my vestibules are open, that would not work.

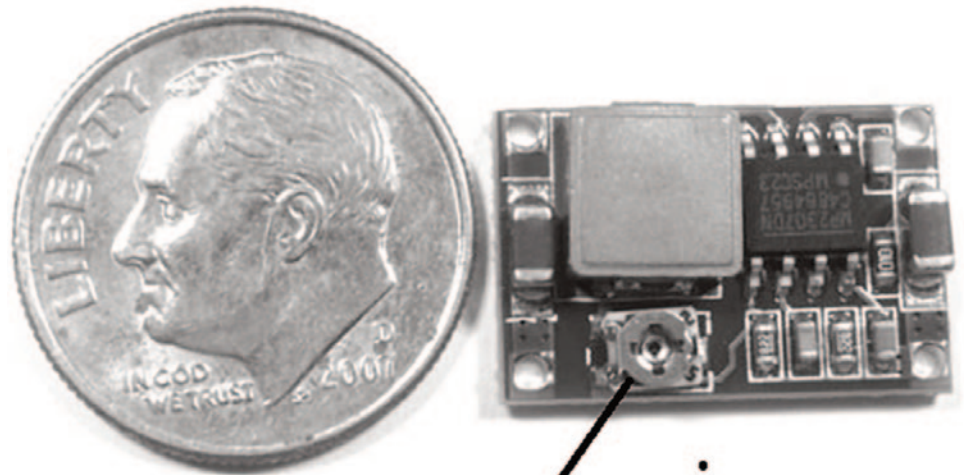
After asking for help on a few forums, I was suggested a solution in the form of a “buck module” (no “booster”) that allows for adjustable peak voltage and will allow current until a certain pre-determined level, in the case of my LED strips, around 8VDC when the LEDs a nice brightness

level. That is about the maximum voltage I use to run a train. I was led to a site for these:

[https://www.ebay.com/sch/i.html?\\_productid=1402279667&rt=nc&\\_pmc=true&LH\\_ItemCondition=3&LH\\_BIN=1&\\_fosrp=1](https://www.ebay.com/sch/i.html?_productid=1402279667&rt=nc&_pmc=true&LH_ItemCondition=3&LH_BIN=1&_fosrp=1)

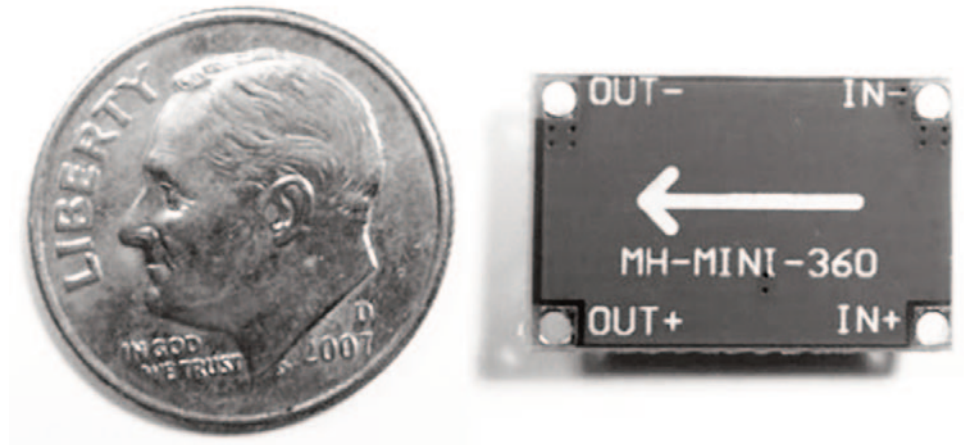
Notice the size! Extremely small and exactly right for my application. Here is what they look like in real life:

The little circular item with the cross in it allows you to vary the peak voltage. I wired up the circuit with the capacitor and rectifier in place, then hooked a voltmeter to the end of the LED strip and adjusted it



**voltage adjustment**

Above is the top side view of the “buck module” and below is the reverse side of it compared to a dime.



# box store works for modelers' use

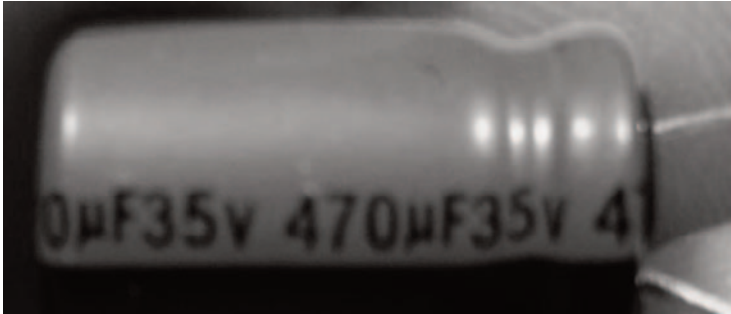
to 8VDC which looks about right to me.

At the full 12VDC, it almost blinded me! Remember, these are for home use

under cabinets. Both the rectifier and capacitor are wired to the proper polarity of "in" holes. Be sure to observe polarity of

the LED strip to the "out" holes.

See LED lighting on page 12



Pictured above is the 450µF 35V capacitor and the photo on the right is similar to the 110VDC 1500mA rectifier that Jerry used. This will give you an idea of the size so it is small enough to fit in the wash room of the passenger car.



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# LED

# lighting from big box store works

Continued from page 11

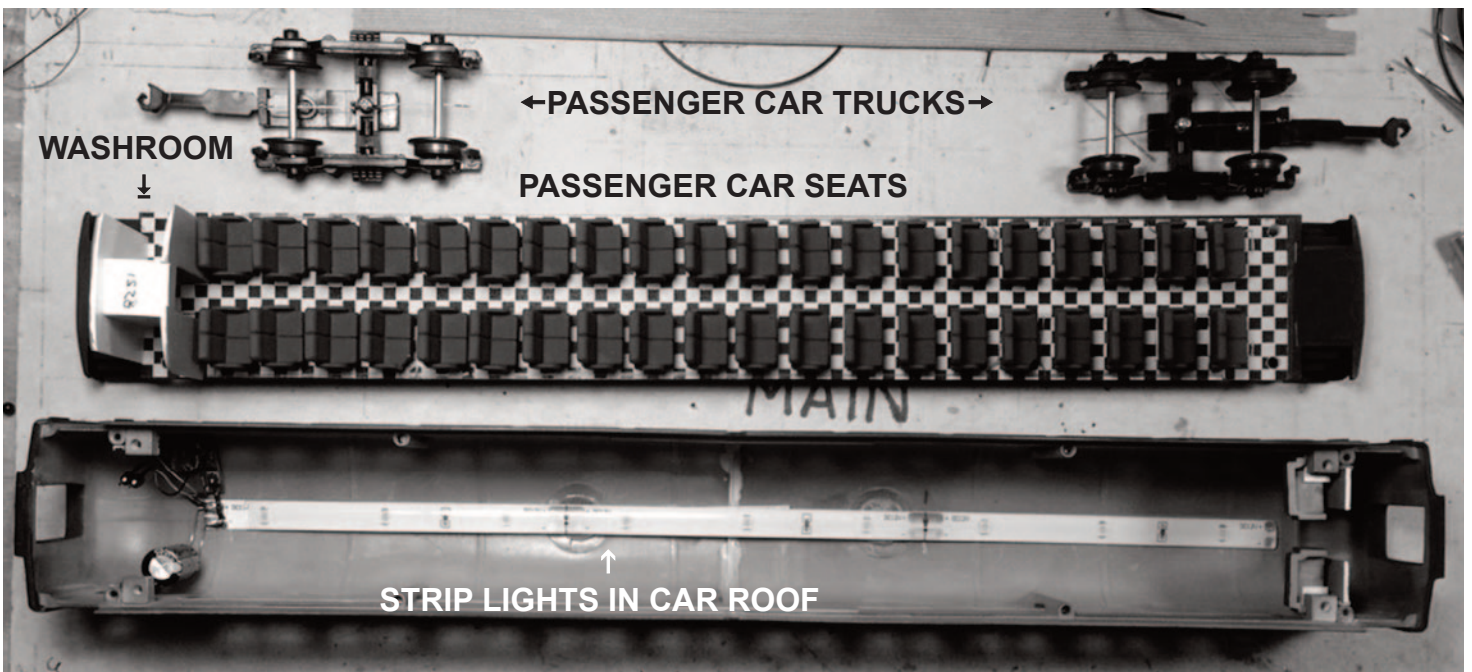


Here is a close up look at how the components fit into a small space inside the car where the washrooms would be located.

Once components installation is complete, then insert the fake washroom facilities for the passenger car so that all parts and wiring are hidden.

*Instructions make project less daunting*

And here is the car with the LED strip and circuitry in place. You can see the lines across the copper areas where you can cut the strip without damage to the circuitry. Each cut strip can then be powered.

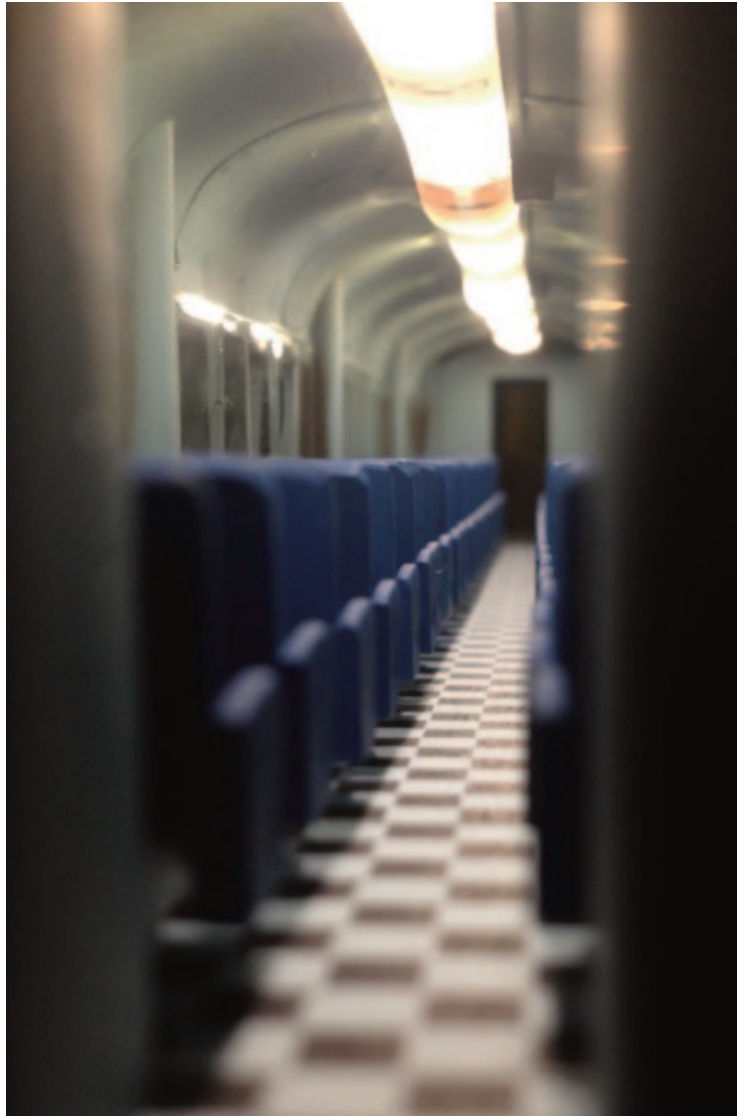


With the LED light strip in place it is easy to see how it will light the entire passenger car and provide a more realistic appearance on your model railroad layout. Not a bad project for an evening or two and it requires just some basic skills.



Components once installed and hidden by the wash-room facade will not be seen after you install frosted windows (ABOVE).

Looking from the car door toward the other end you can see how effective the newly installed LED lighting strips help to make it appear more realistic. Even the strip appears to resemble 4 foot fluorescent tube lights. Just needs some seated passengers and a conductor collecting tickets (RIGHT).



## LED lighting

continued from page 11

When installed and the car assembled with frosted glazing in the washrooms, you cannot make out the components.

I used Miniaturics two pin connectors

to allow removal of the shell if necessary, in the future since I eventually plan on populating the cars with figures. There are other connectors out there, but these are the nicest ones I have found. I also soldered a small length of superflex wire to the ends leading to the truck wheels to allow flexing without breaking when the axle wipers and

trucks swivel. In my case, I used American Models trucks with their axle wipers.

I initially had some concern about heat build-up with the LEDs, but after leaving it on for a LONG time, there was no heat at all. So, I simply used the self-sticking adhesive on the back side of the strip to keep it in place directly on the shell's interior.



Lighted coach only needs some passengers to make the scene even more realistic.

# Product Review

## *The Wait is Finally Over... But the New High-Quality Legacy Berkshires Are Already Sold Out!*

**Will Holt, photos by the author**

When Lionel announced the American Flyer FlyerChief Berkshires in 2013, there were promises that there would be future versions with even higher detail, and equipped with multi-control system electronics.

The wait is over, but word of the increased quality the Leg-

acy Berkshires spread so quickly that Lionel has sold all road names already. Some dealers have a limited stock available, but others are completely sold out. Hopefully, Lionel will produce some more engines, with additional road names and numbers, in the not-too-distant future.

In December 2020, the first run of Legacy/High Detail Berkshires was delivered to S model railroaders. They were

worth the wait. My only unfulfilled wish would have been for scale-wheeled versions of these locomotives. The new versions feature more add-on details than were expected.

The change from FlyerChief to the multi-system Legacy board is most welcome, especially for Hi-Railers who have been using DCC or Legacy for their layout's operations. Included is a pair of spare traction tires (Lionel Part Number

6408082206), a socket wrench to remove the side-rod screws when re-placing the tires, and an eye-dropper for filling the smoke unit. The Legacy board has the same functionality that American Flyer Legacy locomotives have had since the introduction of the ElectroMotive SD-70ACe.

Locomotives operate on conventional AC, Legacy Command Control, and NMRA Standard Digital Command

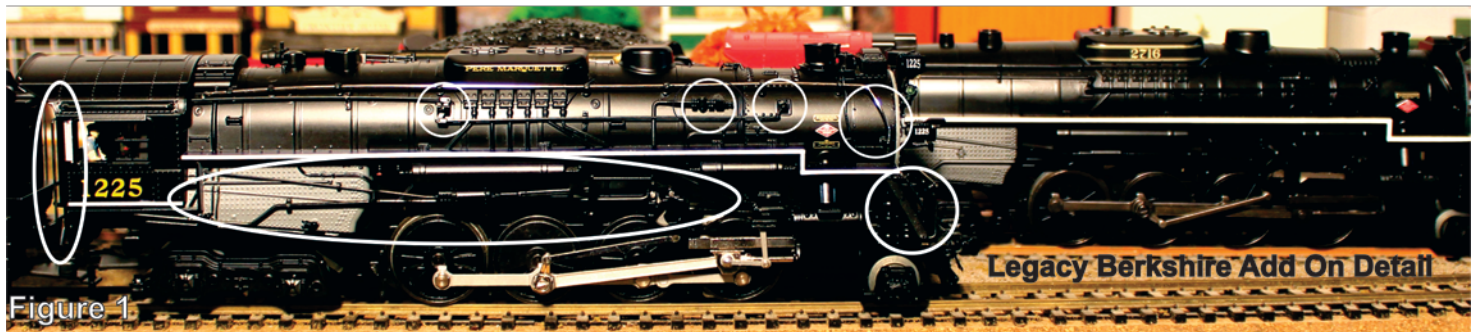


Figure 1

Figure 1 is a side view of the Southern Railway FlyerChief Berkshire behind a Legacy Pere Marquette (all areas of improved detail are circled in white). Some details, such as the sand pipes between the sand dome and the drivers, remain cast on. Handrails on the length of the boiler, with stanchions, are now separately applied. Other

pipings and details, especially below the running boards, are added. The rear of the cab has handrails, and, in both versions, the pilot truck side frame appears too close to the rail head, probably because the bearings inside sit a little low.

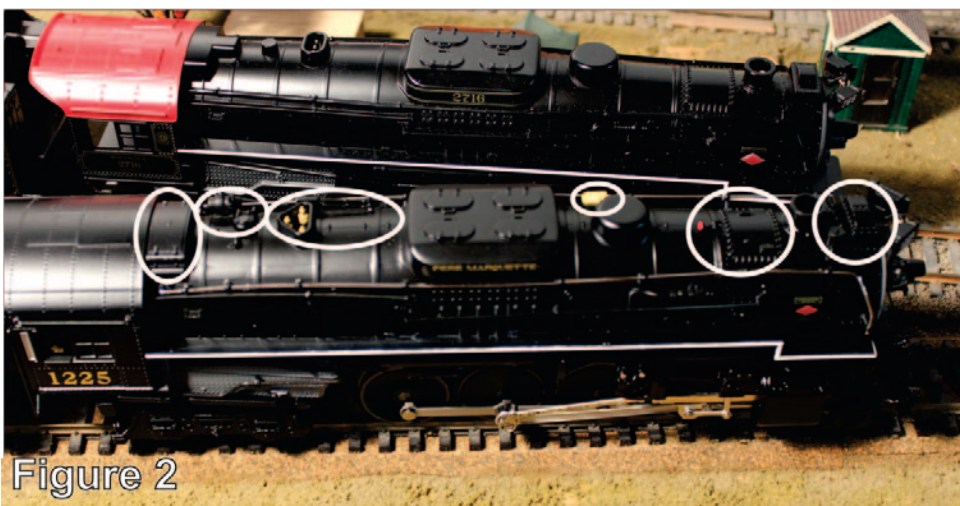


Figure 2

Figure 2, showing the overhead views, is where the differences to the boiler are most obvious. After the added detail ahead of the exhaust stack, more detail is on all four sides of the hatch just behind the stack. Toward the rear, the whistle, the bell, the generator, valves handle, etc. are now separately applied, some with brass plating. The area of the safety valves is changed from an oval enclosure with a straight alignment of the valves to a triangular enclosure and placement of the valves (Photographs in the 1947 Locomotive Builder's Cyclopaedia reflect this arrangement as accurate for the Berkshires of the Nickel Plate Road.)

Control (DCC), with control of motion, sound, and lighting. The locomotives ran well in all three modes. Legacy provides the easiest of control of all of the sounds and lighting of the locomotive. With DCC, the control of sounds is somewhat diminished, and in conventional AC operation mode there is only basic control of sound and lighting. The engines move smoothly, with particularly good low-speed operation.

As a die cast locomotive with traction tires, the pulling power is excellent. One test

train I used was with Gilbert era freight cars, all equipped with the high-friction sintered iron trucks. In conventional AC, the sample tested pulled 20 such American Flyer cars with ease. In DCC operation, which operates at lower voltage, the total was only 15 cars. By comparison, trains hauling cars with low-friction trucks (like American Models, S Helper, and MTH) could pull 35 to 45 cars.

There are also additional lighting effects. The headlight is complemented by illumina-

nated number boards, and the backup light operates when the locomotive is in reverse. A glowing, flickering fire box is visible from the sides and inside the completely detailed cab interior. The sound quality is also improved: all the sounds (exhaust, bell, whistle and voice) have noticeably better low-frequency response, and are crisper.

Improvements in the appearance of the Legacy Berkshires compared with the FlyerChief versions are extensive. Lionel

altered the tooling for the FlyerChief engines in a way that will allow production of both versions in the future by using interchangeable inserts. The basic tooling for the boiler and the tender shells is shared, but the Legacy Berkshires provide a greater value in detailing, lighting, and sound over the FlyerChief versions. In my estimation, the list price differential of only about \$230.00 makes the Legacy version well worth the additional investment.

Figure 3 illustrates the tender differences. The tank deck has applied handrails with stanchions. All four corners have handrails. The rear frame beam includes coupler lift bars and handrails. The top of the rear tender ladder is a stamped metal part, as are stirrup steps on all four corners of the tender. A bonus for Hi-Railers is that the brackets and screws to mount Kadee S couplers on the tender are included.



Figure 3

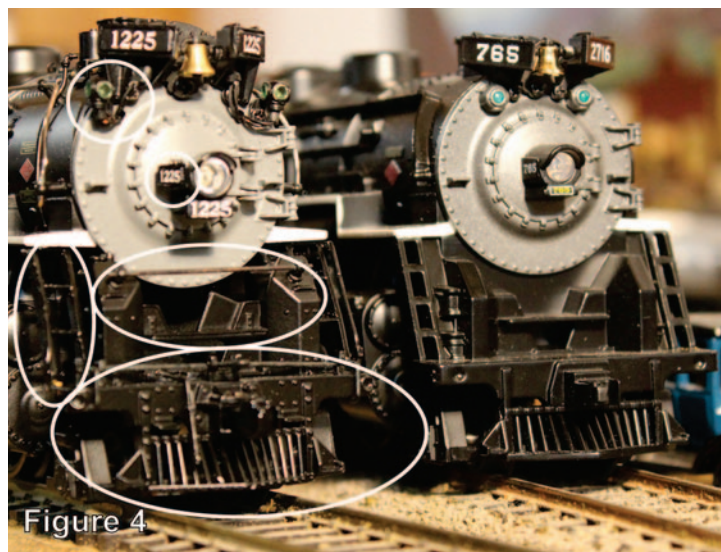


Figure 4

Figure 4 presents the extensive changes in the front view. There is more detail on the shields. The pilot is open between the vertical pipes. The scale-sized dummy coupler, with lift bars, mates with Kadee S scale couplers. Handrails are atop the pilot beam and across the shields. The ladders from the running boards are separate, finer castings. Additional details are on the smoke box front. The headlight and number boards are improved castings. The marker lights are applied separately and light both front and sides.

Figure 5 depicts the extensive detail in the cab. The detailed cab interior includes representations of piping and gauges. Valve handles are highlighted in red. Both an engineer and fireman figure are in their seats. The rear arch of the cab is present. The holes in the firebox door allow the flickering fire to be seen.

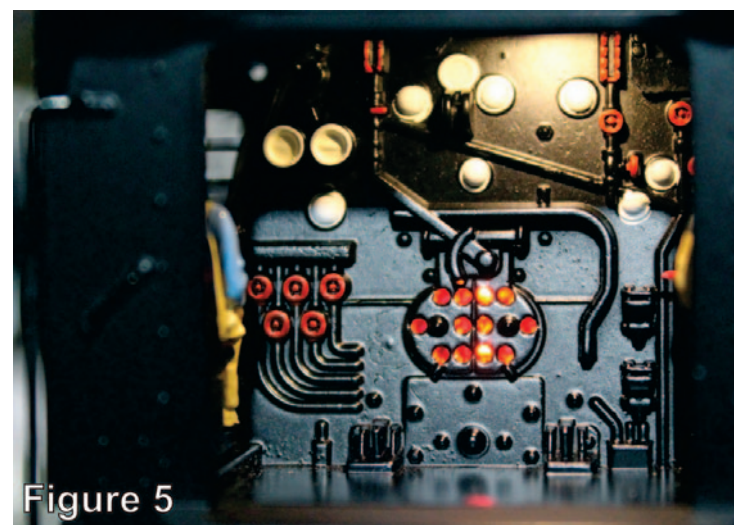


Figure 5

# Unique Kit allows modeler t

**Gaylord Gil, photos by author  
(some enhancements by Brooks  
Stover)**

## *Introducing The 1-Kit*

There is an interesting S scale structure kit called The 1-Kit, which some of you perhaps have in your inventory. If yours is still unbuilt, there's a good chance you're not quite sure what to do with it. Here's the story of my experience with the kit, including some ideas you might use.

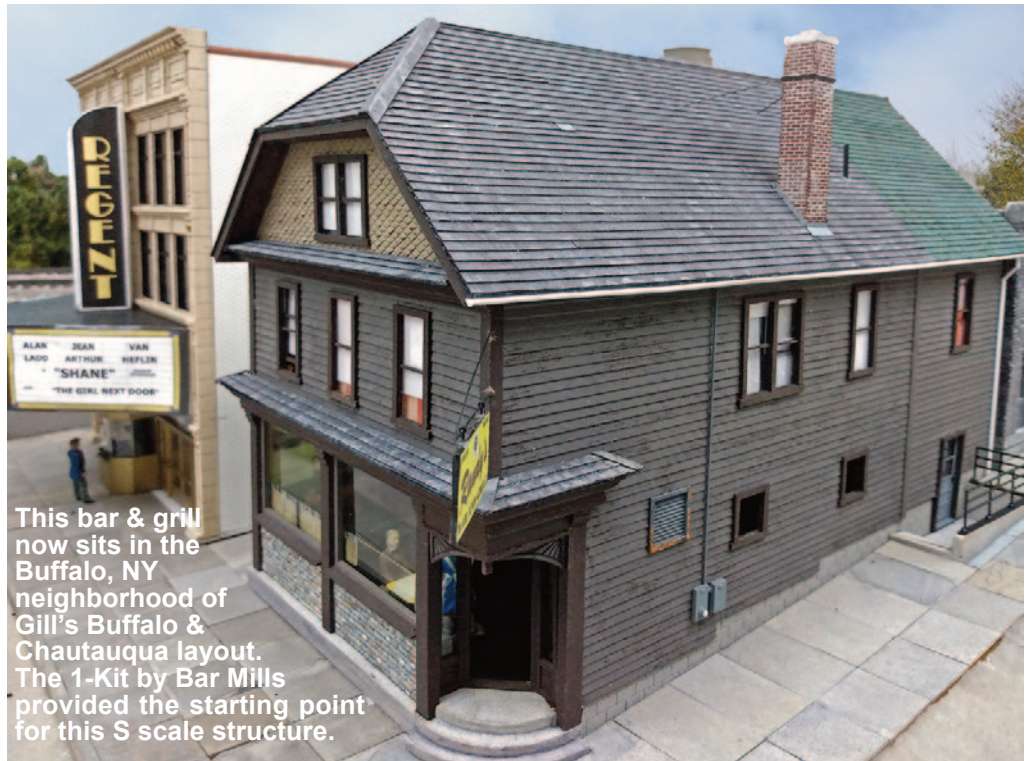
The kit is offered by Bar Mills, a company known for its craftsman-level structure kits in multiple scales. What's unique about the kit is that it doesn't build a particular structure; instead, you create your own design and then adapt the kit's components to complete your model.

The key contents of The 1-Kit consist of panels of milled basswood representing clapboard siding, and the backs of these are cleverly etched with guidelines for window and door openings. The kit also includes laser-cut sheets of windows and doors sized to fit those openings. For a photo of the contents, a more detailed description, and a link to the Bar Mills site, see the Product Gallery at the NASG's excellent website: [www.nasg.org/SProductGallery/ProductGalleryStructures.php](http://www.nasg.org/SProductGallery/ProductGalleryStructures.php).

Given the generic nature of The 1-Kit, it is understandable that a modeler could be a bit intimidated by it – you might feel like an artist staring at a blank canvas, or a writer with the cursor blinking on an empty screen. After I describe my project, I'll mention a few ideas that might help you if you're really stuck.

## *My Project*

In my case I already had an idea in mind for a structure I wanted to build. Part of my Buffalo & Chautauqua layout represents PRR trackage through the city of Buffalo, NY, and adjacent to my large yard



This bar & grill now sits in the Buffalo, NY neighborhood of Gill's Buffalo & Chautauqua layout. The 1-Kit by Bar Mills provided the starting point for this S scale structure.

there is a neighborhood of commercial buildings. For a few years I've had a mock-up sitting in that space, at the intersection of two streets, and I had been thinking a restaurant would be an appropriate business to place there. Up to this point I was imagining a flat-roofed brick or tile structure, and I expected I would be scratch-building it.

I acquired my copy of The 1-Kit early in the coronavirus pandemic, and I immediately saw new possibilities. During prior research I had located a website called Forgotten Buffalo, and this site yielded a great candidate for my purposes: Arty's Grill. The website had both a current photo

and an older one from probably the 1920's (photo 1). Arty's would be the inspiration for a unique bar & grill on my layout.

## *Wall Construction*

My first step was to lay out the front of the building, and I focused on the second story containing three equally-spaced windows. Using the photos of Arty's I estimated the width of the wall, and then looked at the back of the kit's clapboard sheet to see how the laser-etched window pattern would work. These etched guidelines make openings which are 32 scale inches wide, with five stacked "knockouts" which are each 18 scale inches in height.

For my structure I determined that removing four knockouts would bring my window openings to the height I wanted, but the spacing between windows wasn't right. I cut out my openings by using the guidelines as much as possible (photo 2).

With the front width of the build-



Photo 1 - Above are two Internet photos of Arty's Grill.

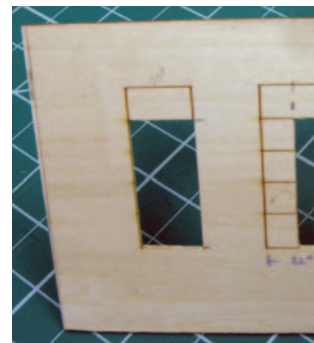


Photo 2-The clapboard etched patterns on the kit. Cutting out window and door



# to create on a blank canvas

ing now fixed, I scratch-built the first story: strip wood for the window framing, mat-board for the lower part of the wall, Grandt Line door and strip styrene for the entrance, and sheet styrene for the quarter-circle concrete steps. The third story of the front was fashioned from mat-board with window openings matching those of the kit. Similar to the vintage photo of Arty's, I added diamond-shaped shingles (HO scale) as siding on this story.

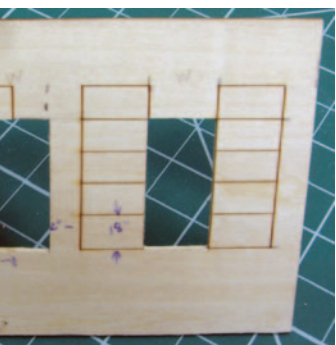
Using more of the clapboard panels that the kit provided, I next laid out the long side wall. Because of the length I wanted, I had to splice two panels of clapboard. I chose to represent that the original building had been shorter, and later an addition was built at the rear, so I could hide the seam with a piece of vertical trim.

Time to mention one of the time-saving features of The 1-Kit: for this wall of the project, the placement of the pre-etched window and door openings were generally simply fine for my purposes. In using the etched guidelines, I knew that the openings would be exactly the right dimensions to receive the kit's windows and doors. I trimmed the kit's window sashes to the sizes I needed and then glued them to their frames.

## Painting

With the walls and windows coming together, I wanted to begin some of the painting. My wife Susan is very good with decorating and selecting colors, and after I showed her the vintage photo of Arty's, she suggested I go with a two-tone brown scheme.

For the darker trim I selected an earth brown color (matte finish in a spray-can), and I mixed the lighter wall color with acrylic craft paints. Pre-painting all these pieces before gluing them together is the way to go: photo 3



Walls come with laser-etched window and door openings.

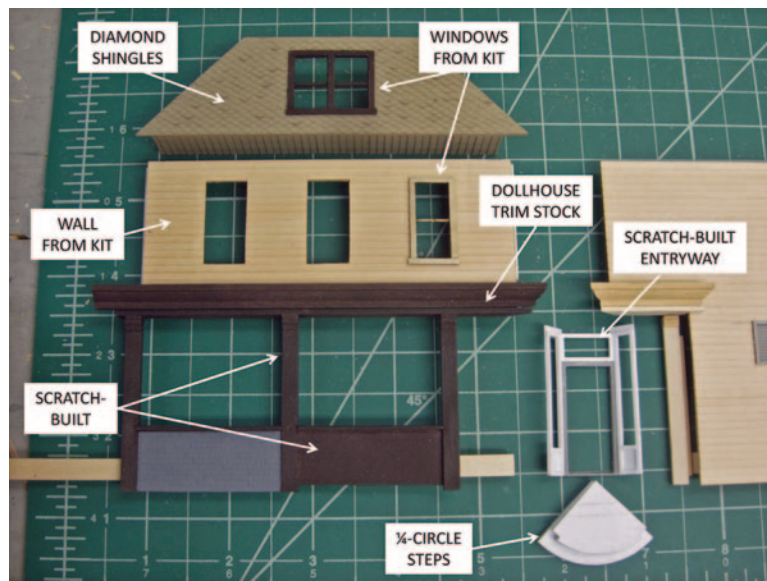


Photo 3-These are the components which form the front wall of Gaylord's structure. While the kit provided materials for the second story, the uniqueness of the first and third stories required scratch-building.

shows the front wall components laid out, with the first floor framing painted.

To represent a somewhat-neglected structure, I stippled the paint on the walls so they appeared as if portions of the paint had weathered and flaked off. For the long side wall I emphasized the distinction between the original structure and the addition, with the addition getting a fresh coat of paint.

Since The 1-Kit provides no foundation materials, I did the foundation for this wall as cement block: laser-etched sheet made by Rail Scale Models. Photo 4 shows the side wall assembled and painted. Note the stair-step notching at the bottom: the secondary street running along this side rises to the rear at a 50 grade, so I cut the siding accordingly.

## The Roof

Roofing materials are likewise absent from The 1-Kit, so for this project I chose mat-board in a 1/16" thickness. The clipped peak of my roof was a

feature I had never created before, but through experimentation I arrived at the correct dimensions of the triangle at the front of the building.

When it came time for adding shingles, I adopted a technique used by my good friend Brooks Stover. I glued a sheet of Clever Models paper shingles to black cardstock,

then applied double-sided transfer tape

See Unique kit on page 26



Photo 4-The aging and peeling paint effect was carried through to the side wall, except on the rear addition, which has a fresher paint job. The trim pieces were painted before gluing.



Photo 5-Here's the completed roofing job, using self-made shingle strips, and the rear addition has newer shingles in green. Gaylord added light weathering across the roof.

# Simon Parent 3-D prints a CNR class N-3 Consolidation

Jim Martin, photos by Simon Parent

It has become evident over the past few years that the advances in 3-D printing are driving yet another renaissance in S scale: not a top-down renaissance, but one that is driven by the hobby's base of talented, computer savvy modelers.

Value-for-dollar improvements in the printers, software advancements, and most importantly for larger scales, larger printing beds, will help S scale to do what it does best...survive.

Simon Parent has been at the sharp edge of the hobby for years, building his own machine tools; designing photo etch artwork; building master patterns; lost wax casting his own brass detail items; and laser cutting his own structures and track templates. Therefore, it comes as no surprise that he has been doing his own in-house 3-D printing, experimenting with various techniques and materials to make the best use of this evolving technology.

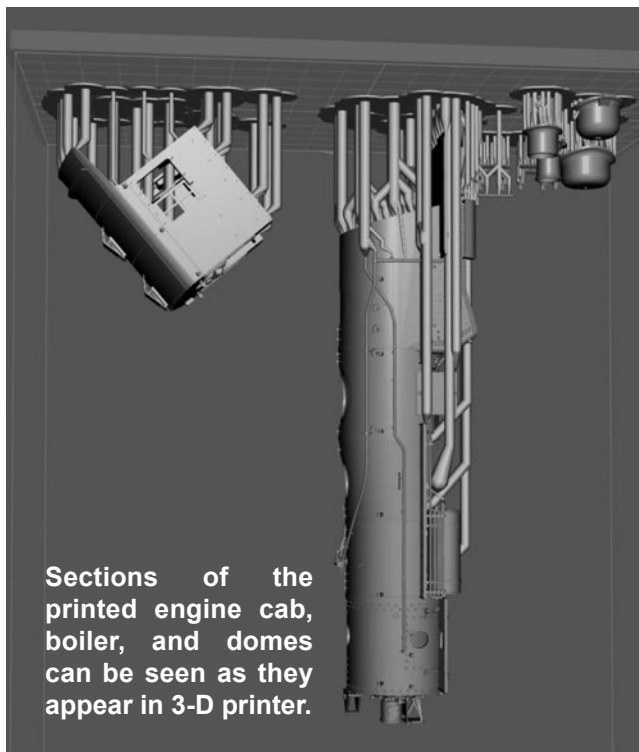
Simon recently moved up to a Anycubic Photon Mono X printer which, can print as fine as .002" in the by plane (back and forth) and .0004" in the z (vertical) plane. It is also considerably faster than his previous printer. Unlike filament printers, this is a resin printer. UV light is introduced onto a submerged printer bed. The light hardens the resin layer by layer. Simon is experimenting with different resins, but this project used the Siraya Tech brand.

Of course, none of this can happen without marching orders. The printer takes its orders from drawings Simon prepares using Sketchup Pro 2020

Before we go any further, a quick side note: If a lot of this baffles you the same way it baffles me, just remember...one does not have to be clever to take advantage of today's 3-D printing...one only must know clever people.

Now to Simon's newest project, this 3-D printed model of a CNR class N-3 Consolidation. Viewed in order, the pictures themselves do a rather good job of telling the story of how this model progressed from computer to printer bed to track-ready.

What made this project possible was the Anycubic Mono X's ability to print objects up to 9.5 inches tall. What the pictures do not show are the number of experimental layouts Simon tried to determine the optimum print configuration. Among his considerations was the orientation of the components to best hide the print layering. Layering, although fine, still must be planned for. Simon also wanted to minimize the number of separate components. The tender is just two large pieces while



Sections of the printed engine cab, boiler, and domes can be seen as they appear in 3-D printer.

the loco itself consists of the boiler with boiler front, the cab, domes and stack, and the detail parts along and under the running board.

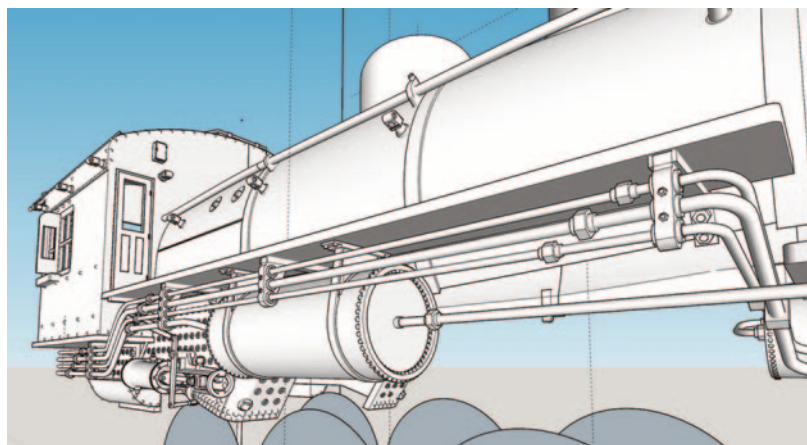
All this 3-

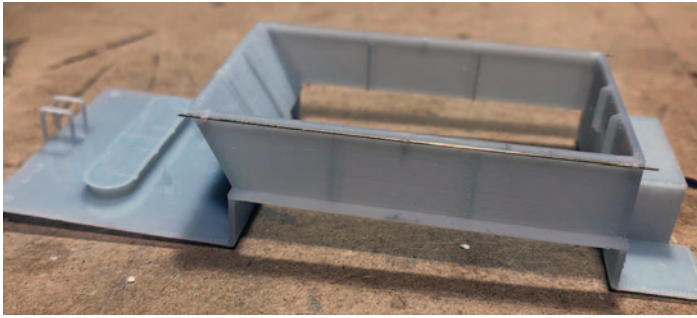
D goodness sits atop a modified S Helper Service 28-0 engine and tender frame.

This is a project Simon has done for himself. Is there a commercial potential for this project? There are a lot of question marks. First is speed of production. It can take up to 15 hours to print just the loco superstructure. The printing speed is related to the height of the part and the z resolution. The tender is yet another separate printing. Could the 3-D prints serve as masters for resin castings? Yes, but the process would be quite a bit more involved than a flat-cast boxcar. These are all questions for another day and another article. In the meantime, we hope you enjoy seeing what Simon has accomplished.

Simon Parent is a member of the Canada-based S Scale Workshop which has been invited to show its modular layout at this summer's NASG convention in Buffalo August 3-7. Simon will be sure to have this loco with him, so attendees will be able to get a first had look at this remarkable 3-D printed model.

Till next time, Cheers eh?





Above left is the top of the tender and above right is bottom half with the completed tender directly to the right. Below left is the front-on view of the locomotive and the picture to the right show the detailed side of the machine.



## The Morrell Steel Reefer and the SOHIO Tank Car

were never before offered in S scale Hi-Rail. These classic designs were originally offered by A. C. Gilbert/American Flyer only in HO. Our club has arranged for American Models (Morrell Reefer) and Lionel (SOHIO Tank Car) to produce them exclusively for S scale modelers. And they can be yours for \$70.00 each, including shipping within the continental U.S.

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**Southern California S Gaugers' Club – [www.socalsgaugers.org](http://www.socalsgaugers.org)**

# Modifying on an old master modeler's layout design leaves original features with a few modern twists

**Jeff Madden, New Products Editor**

The passing of Frank Titman several years ago reminded me of his original Delaware and Western hi-rail layout that I had always admired for its operational scheme. I have poured

over this plan (1959 Model Railroader and the S Gauge Herald) many times and have often thought of updating it a bit so that access was improved.

The genius of the original D&W plan (included in this article) is the use of a single staging yard serving both ends of a fictional Appalachian type railroad. An

additional bonus is the use of a wye to turn locos and trains that serve the staging yard - thus, no need for a space-wasting turntable and roundhouse. Note too that the engine house and loco servicing area is useable to let locos access the staging yard for trains going in either direction.

The overall design is basically linear

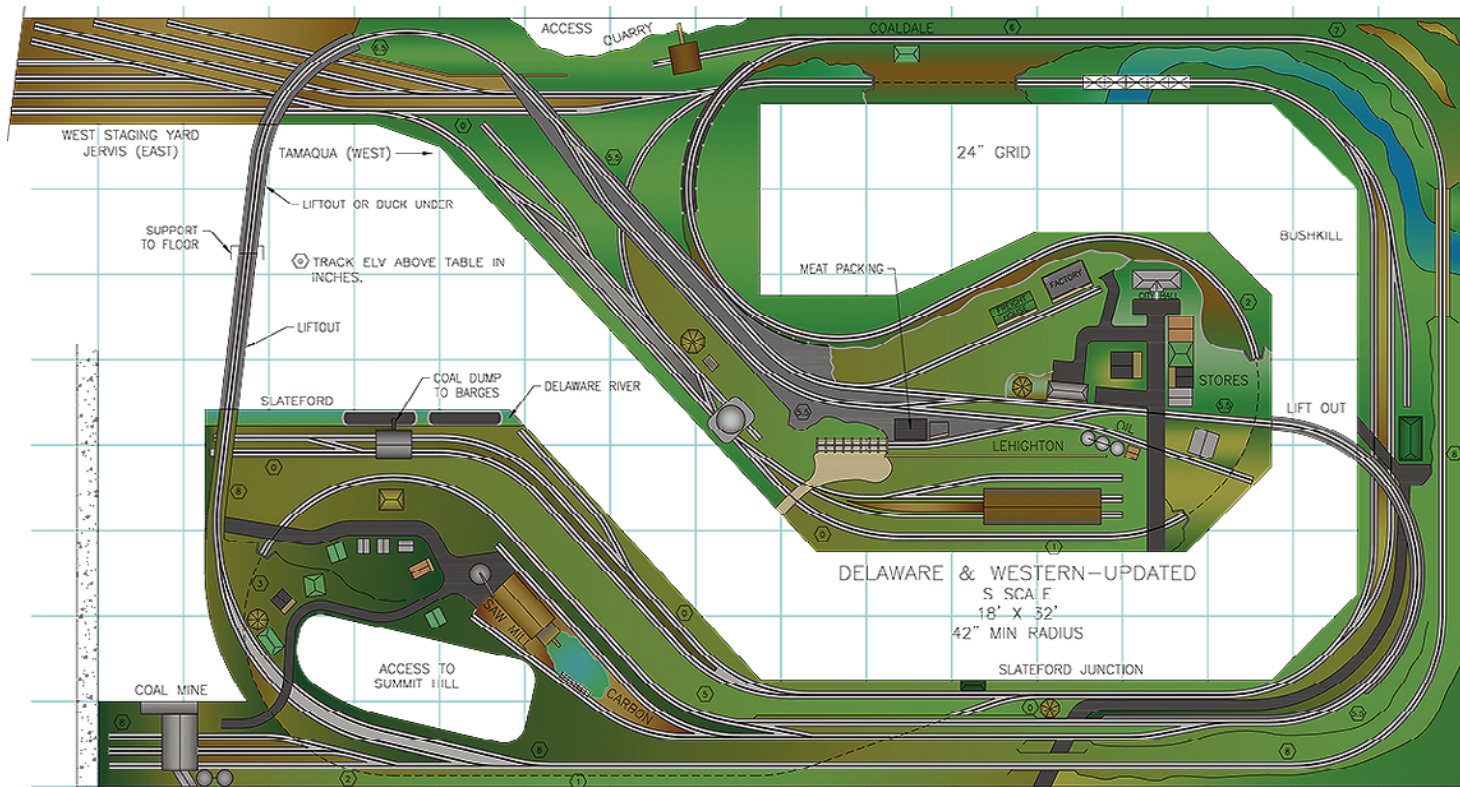


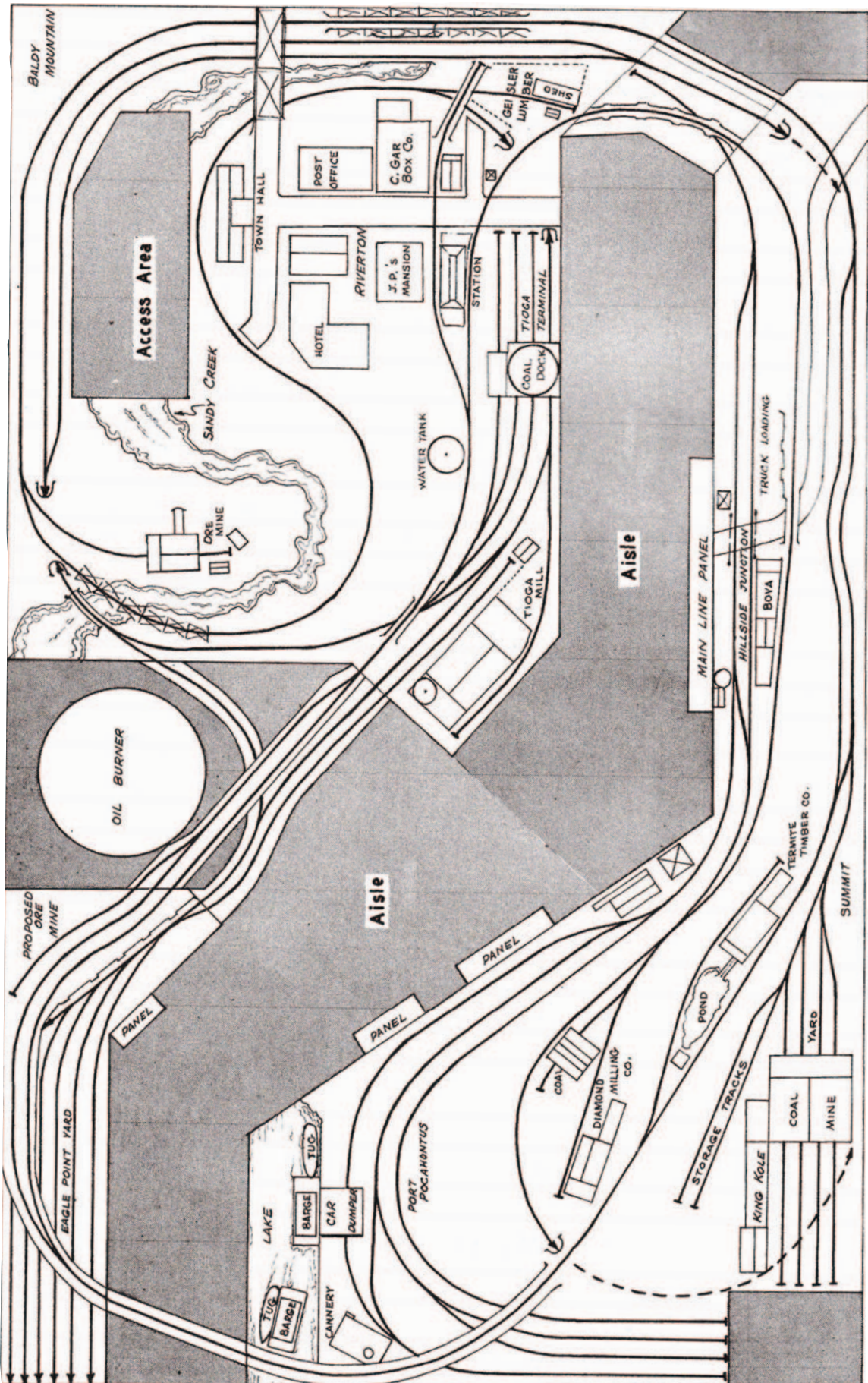
illustration by Joe Kimber

around-the-wall with one peninsula and a turnback area. On my update plan I created a peninsula for better access as Frank's original D&W had only a pop-up hole in this area. I did add one pop-up (hidden by tree and hill view blocks) in the turnback area above the barge and sawmill sidings. I did, however, design in a narrow access aisle beyond the turnback area to access the mine tracks.

Yes, lift-outs are needed in two areas, but once inside of the longer bridge span most trackage is reachable from the main aisle space. The short, curved lift-out by Bushkill only need be used to reach the freight house siding there and the quarry siding at Coaldale.

Once out on the mainline – proceeding from either direction on the wye from the staging yard – there are 4 passing sidings to use for switching or holding another train. The principal traffic makers are the coal mine, quarry, and sawmill, typical of Appalachian railroading.

Let us take a run east from Tamaqua (staging yard west) with some coal empties. We might have Reading or  
**See Improvemnts on page 25**



ORIGINAL DRAWING BY F.R. TITMAN

NOT TO SCALE

# Conversion

## *How to change an American Flyer flatcar (metal or plastic) to scale*

**Edwin C. Kirstatter,**  
photos by the author

The early American Flyer 45-1/2 foot flatcars are pretty close to scale replicas. The early ones were die-cast Zamac, a hard zinc alloy. The later ones were molded in several colors of polystyrene plastic. They came in several variations: some have open stake pockets for tying down loads, others have some small detail differences. I would rather work with the plastic ones, but I have one of the older cast metal ones to convert to S scale appearance and operation. I do not like to use this metal body because it is so hard to work on, milling off unwanted parts and scraping off the eight cast-on grab irons and drilling holes to accept wire grabs.

Before starting on this proj-

ect, remove the old lettering and paint from these bodies. If metal, use lacquer thinner; if plastic, use Scalecoat II "Wash Away." After removal, wash in warm detergent water, followed by a water rinse.

Use the ACE bolsters with



"A" end, added fillers to close the space beside coupler pockets.

coupler adapter for this conversion if you can find them. If not, make bolsters from 1/4" wood cut to fit and shaped to clear the truck side frames. Remove the cast-on grab irons from the ends and from the ends of the sides. Drill holes for the new Tichy grab irons and push them in. If the crude cor-

ner sill steps are broken off, that is OK; we were going to remove them anyway to put on scale-looking ones made from flat wire staples re-bent to 12" scale steps. Drill holes for them and cement them in place. Or you can file the cast-on steps to

a finer contour.

To mount scale trucks and couplers to the plastic cars, mill off the Flyer truck mounting pads with a Dremel hand tool. Then cut and shape new bolsters from 1/4" square wood from a craft store. The bolsters should be glued to the underframe so that the truck centers

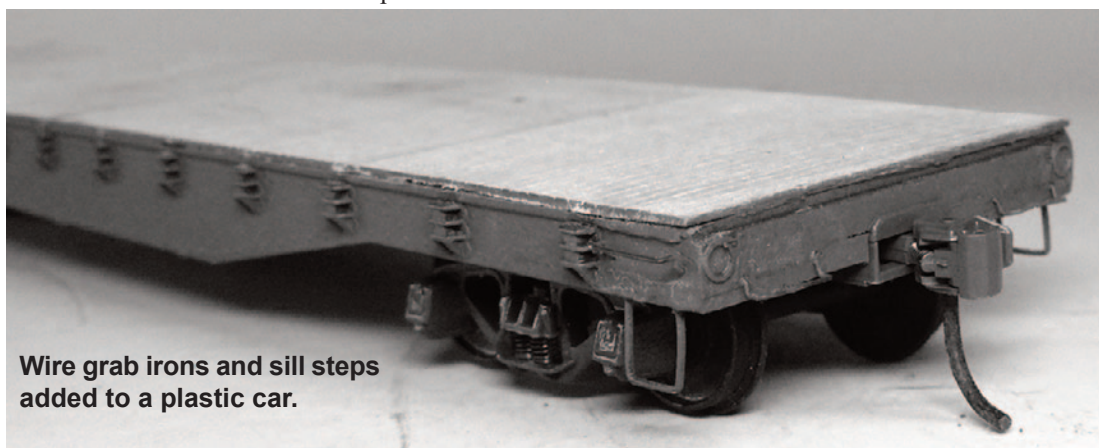
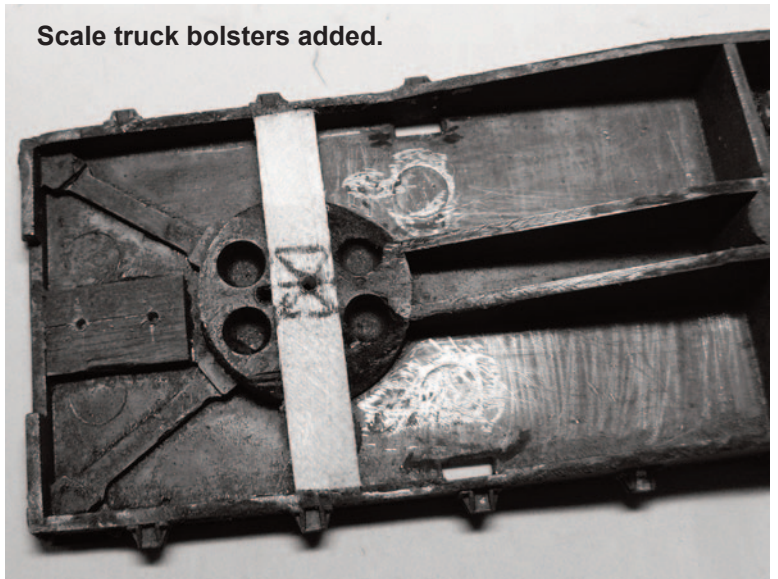
will be spaced 34 to 36 scale feet apart. (I recommend 70-ton Bettendorf type trucks.) Drill truck mounting holes with a No. 50 drill bit and attach the trucks with #No. 4 flathead wood screws 1/2" long. Mount your Kadee couplers on a .070"

thick wood or plastic pad fitted between the end sills and the new bolsters. Now check the car height with a NASG coupler-height gauge and adjust if/as necessary. If the car rides too high, file the bolsters' truck-mounting surfaces. If too low, use washers between the bolsters

and the trucks, or glue shims under the couplers. You may now glue plastic filler pieces beside the coupler pockets at the end sills to fill the open spaces. I used .040" x .100" strip styrene for this. Now remove the trucks and reinstall them after you paint and letter the car.

Cover the center sill from bolster to bolster with a plate of .010" sheet plastic. Make this sheet 1/16" wider than the width across the center sill members so as to provide a 1/32" flange on each side of the sill. Now cut a 1/8" wide strip of .005" styrene sheet 1/8" wide. Using a pounce wheel, emboss rivets down the center of this strip. Cut this strip to fit from side to side over each of the three cross-bearers and cement them in place with super glue.

Scale truck bolsters added.

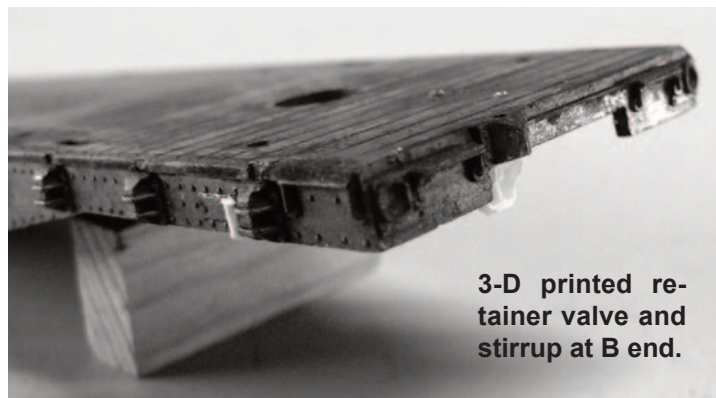
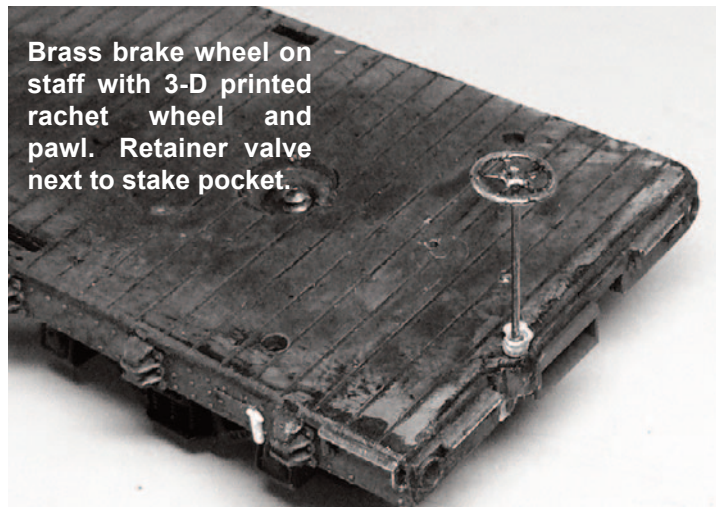
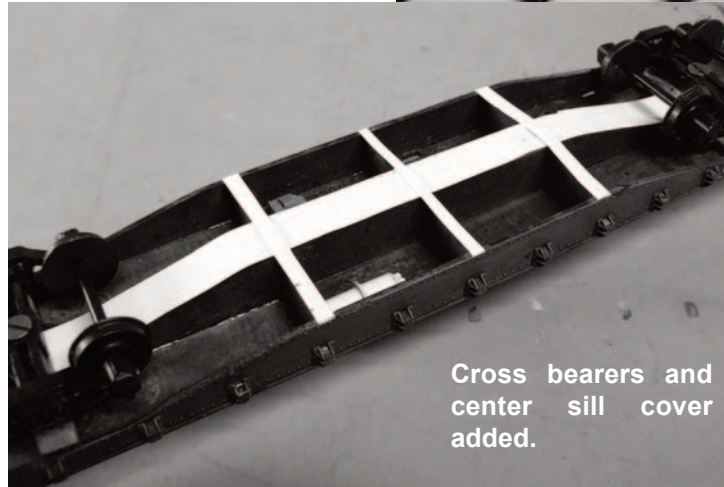
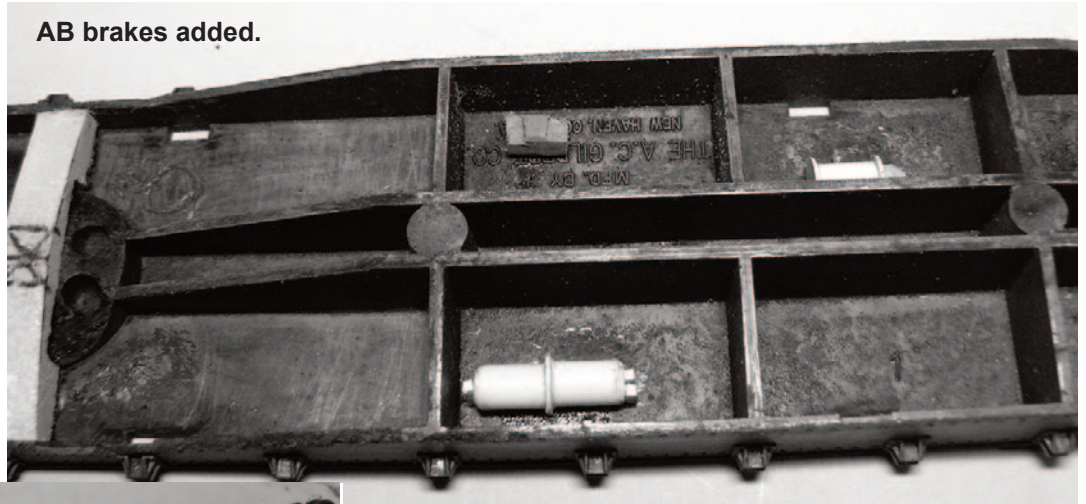


Wire grab irons and sill steps added to a plastic car.

Install a three-part AB brake system under this car if you want them. If you want to add pipes, levers, and rods to model a fully detailed brake system, go for it. I didn't do this because it is quite difficult to do and nearly impossible to see in normal operation.

Add destination tack boards to the sides near where the car numbers will be found. Make these 6"x 8" from .015" styrene sheet. Touch up any imperfections with modelers' plastic putty.

AB brakes added.



Solder a new scale brass hand brake wheel (15" scale diameter or larger) to a .025" phosphor bronze wire staff. Then thread the staff wire into a 3D-printed ratchet and pawl detail, slide it into the brake staff hole at the end of the carbody, and cement it in place such that the wheel is around three to four scale feet above the deck. (This is the "B end," "B" for "brake.") Now cut off the staff flush with the bottom of the car end. Cement a 3D-printed brake staff stirrup centered beneath the brake staff. Install a 3D-printed retainer valve on left side of the B end. Be careful! 3D printed parts are easily broken and must be handled with care.

If you want to install coupler cut levers, bend them from .016" brass wire. Drill four holes in each car end and attach the cut levers with bass eye pins inserted in the holes. Last, add air brake hoses from BTS.

Now with your model finished, remove the couplers,

then clean your model with alcohol. Paint it with the color of your choice. Use a gloss color, else use a dull color followed by a gloss coat to accept decal installation. I spray-painted my model with Scalecoat II, No. 2010 Black. Tichy has several decals sets to use on this size of flatcar and for several roads.

After the painted has dried, put on a new wood deck over the old cast one. This will cover the many holes used there from the different types of Flyer loads as well as the truck mounting holes. I used Northeastern 1/32" thick by 3/32" wide Strip wood, (2"x 6" in S), cut them 1- 7/8" long to slightly overhang the car sides. Use slightly shorter ones at the stake pockets so that the pocket holes are not covered. Ditto for the area above the grab irons at ends of sides. Use a contact cement like Walthers GOO or

**See Faltcar on page 24**

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# Flyer flatcar converted to scale

Continued from page 23

Pliobond. (Alternatively, you could use Northeastern scribed sheet wood to make a one-piece deck, but it would look as realistic as individual boards.)

Weather your deck with an oak stain or paint it Grimy Black to resemble creosoted wood.

Now install your decals. I used a Tichy set 10239S for an L&N 48-foot steel flatcar built in 1927 painted black. Use their special instructions when putting on large pieces of their decals. Dullcote it with Testors #1260 when they are set and dry. Then you can add a load.

The plastic bodies will only weigh three ounces to start with. The metal cars without anything on them weigh seven ounces. Look for a lightweight



Here is the car after the wooden deck has been completed.

load to put on a metal one! Drill out the stake pockets you will need to tie down your load if they are not already open.

The load I used is an HO JWD #11406 "Rusty Flange Pipe Load" stack. These are 22-1/2 foot long 2-foot diameter pipes in S, banded together on wood cleats. This load weighs

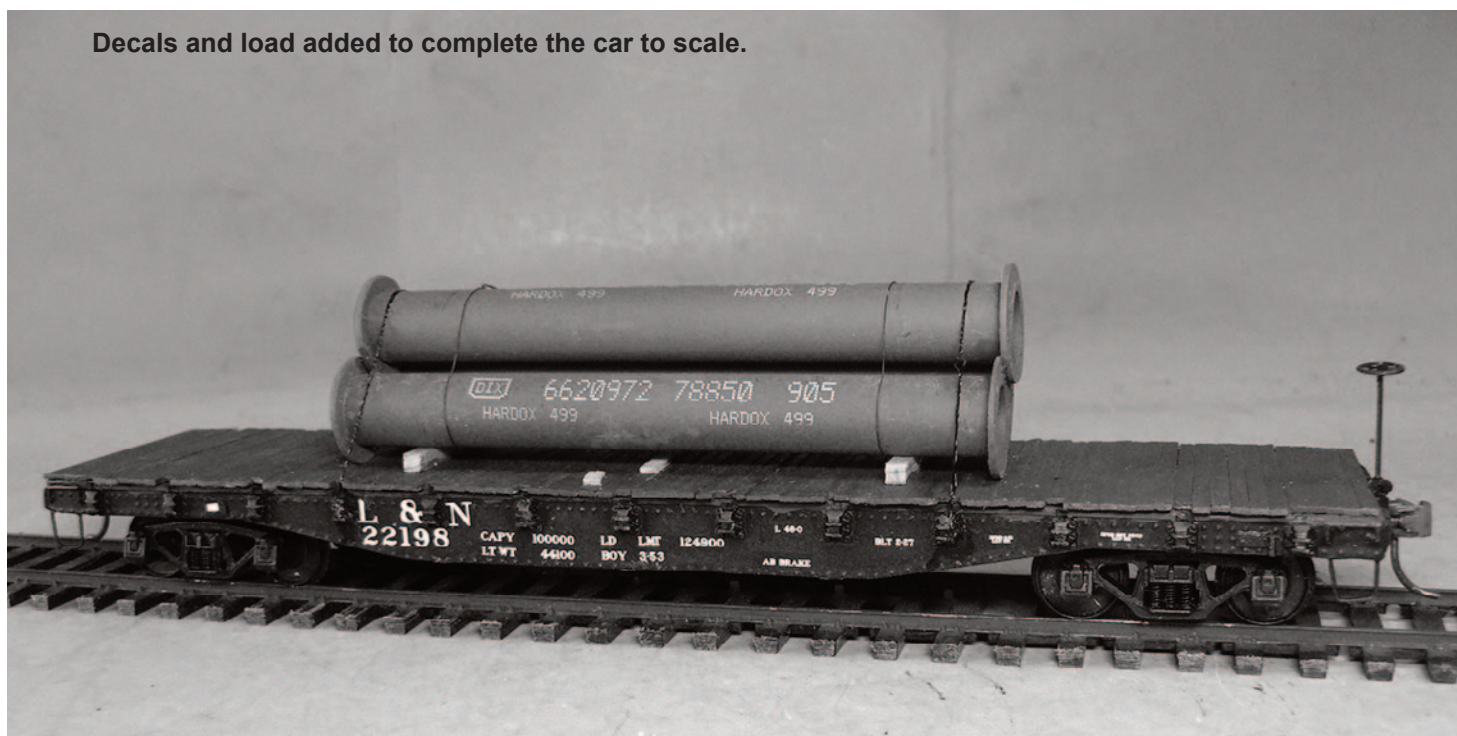
only one ounce. Twist two strands of fine magnet wire tightly together to represent tie-down cable, then cut into two equal lengths. Loop each over the pipe load near the load's end and insert the cable ends into the stake pocket holes. Bring the cable ends back up top and twist them around the

cable above the stake pockets and trim off the excess. Looking at the photo will help you.

Now your Flyer car has been completely converted into a fine-looking detailed model for scale operation on your layout.

**See References on page 25**

Decals and load added to complete the car to scale.



**Readers please note that on page 6 the Deadlines for submitting articles, advertisements and classified ads has changed. Please submit on or before the listed Deadline.**



# Modifying an old master modeler's layout design leaves original features with a few modern twists

Continued from page 21

D&W power coming to us from the engine house (steam or diesel – Frank preferred steam). We head through a tunnel just past the engine house with our train and climb uphill past Coaldale to Summit

Hill, PA. Here we swap out the empties for coal loads. Then it is downhill through Lehighton where we pass a westbound passenger local headed by a D&W Pacific. After crossing the long-curved trestle, we come to Carbon. No work here so it is down to Slateford Jct. Here we shove our loaded coal hoppers into the barge dump track next to the Delaware River. After pulling out the empties we dart through Bushkill and end up at the Erie connection at Port Jervis (east staging).

Local way freights usually run east one day and west the next, working all the non-coal mine industries in Coaldale, Lehighton, Carbon and Bushkill. Then there is probably a highball freight or two hauling bridge traffic between the two main terminals such as blocks of reefers, TOFC and non-local freight that connect between the Reading and the Erie ends of the railroad.

Passenger trains would be limited to one

train each day, plus maybe a doodlebug to handle mine workers from Tamaqua up to Summit Hill and back.

Overall, this is a doable layout for a basement location. The size of the plan is 18 feet by 32 feet with the ability to extend the staging yards – recommended if possible. Except for a mechanical or electrical snafu, only the way freight operators should have to access beyond the short lift-out at Bushkill.

Compare this plan with Frank's original and you will see that his was pretty darn good for the space and the time when it was designed. The original D&W did have tighter curves, steeper grades, and access pop-ups behind Riverton and by the coal mine, but it sure seems to me to have been a fun layout to operate in the day.

The updated plan shows that his original design is pretty much timeless. Thanks Frank!

## AF flatcar converted to scale references

### REFERENCES:

United States Safety Appliances for all classes of Cars and Locomotives, AAR Edition, 1950.

AAR Rules Governing Loading of Commodities on Open Top Cars. 1945.

TFSTrains@shapeways.com 3D printed 3/16" scale freight car parts.

Tichy Train Group, catalogs of decals and parts. www.tichytraingroup.com

### RESOURCES:

Tichy Train Group (grab irons)

Kadee (couplers)

Evergreen Scale Models styrene sheet and strip

TFSTrains@shapeways.com 3D printed 3/16" scale freight car parts:

BTS (brake system components)

MicroMark (brass eye pins)

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# Unique kit

Continued from page 17

across the entire back. Next I cut the sheet into strips which had two rows of shingles each, and this creates peel-and-stick strips which can be overlapped for a great 3-D effect. For the chimney I glued pieces of brick pattern sheet (also Rail Scale Models) around a wood core. I also carried through the idea of the newer addition by switching to a different color shingle. See photo 5 for a close-up of the roof features.

## Additional Details

There were plenty of opportunities to add detail this structure (refer to photo 6):

1. Interior – The large front windows called for a detailed interior. Two groupings of figures were placed on a simple floor panel with a backdrop across the rear made from interior images on the internet.

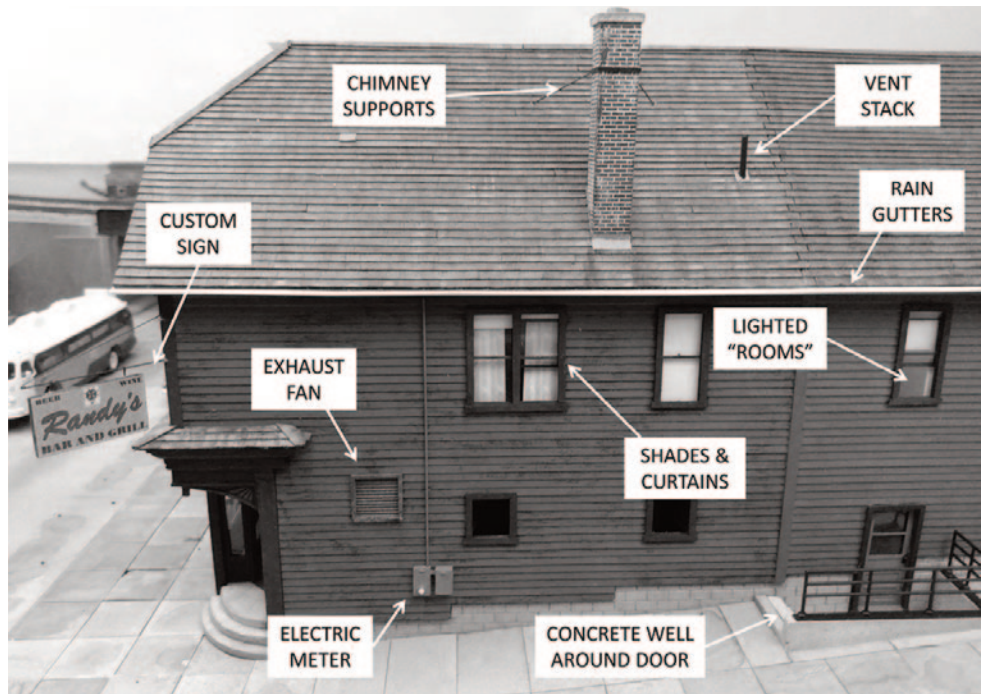
2. Window Treatments – Some window sashes were glued in an opened position, and a variety of curtains and shades were made from images on the internet.

3. Interior Lighting – On the main floor, LEDs create a blue-ish fluorescent light effect. For contrast, two “shadow boxed” upstairs rooms are lit with yellow-ish incandescent bulbs.

4. Stairwell – The rise of the street along the side of the building provided an opportunity for an interesting stairwell down to the door. The concrete walls of the well were made of 1/8” basswood and the railing is Central Valley HO material.

5. Signage – Arty’s Grill was the inspiration for my project, but I created a custom sign in PowerPoint to recognize my buddy Randy Bosscher, an inspirational S modeler. Printed images of the sign were attached to both sides of .040” styrene and hung from .025” wire.

6. Other Details – The louvered exhaust



**Photo 6 - This photo shows the completed Randy's Bar & Grill, highlighting many of the added details. Gaylord feels The 1-Kit gave him a good start on a fun construction project.**

vent is an HO part, the electric meter uses wire for conduit, chimney-anchoring rods are made from wire, and the vent stack is a short length of brass tubing. Gutters and downspouts are made from strip styrene.

## Final Thoughts

With the amount of scratch-building and added materials in this project, you might ask: Was The 1-Kit really much help? I would say that it was. For me, acquiring the kit was a catalyst for getting me going on construction, after looking at my mock-up and thinking about the project for a few years. As I mentioned in the section on wall construction, the kit's pre-etched window and door openings did speed up the process of getting those components in place. And even without those arguments, at the very least the 1-Kit provides you some raw materials for building something.

At the beginning of this article I said I would provide some ideas for other mod-

elers, on the chance you had one of these kits but didn't know what to do with it. Here are my thoughts:

- Be flexible as you consider what you might want to build. In the beginning I had a quite different idea for the style of restaurant I wanted, but once I saw what The 1-Kit offered, I modified my design and abandoned the planned brick-walled approach.

- Think beyond the constraints of the wall panel sizes. It's easy to stack panels vertically to create multi-story structures (think tenement houses). Although it requires more effort to hide the seams, it's also easy to extend the length of your structure (think warehouses and storage buildings).

- Conversely, don't feel you have to use up all the kit's contents on a single large project. There are lots of small buildings, such as garages and shanties, which could

be useful on just about any layout. Build a couple of those and you'll still have plenty of leftovers for something else.

Start to finish, this was a fun project. I overcame a few challenges, tried a couple new techniques, and placed a one-of-a-kind structure on my B&C layout.



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# Calendar of Events

This page is for the listing of model railroad events, with emphasis given to S-gauge or S-scale. If you know of an upcoming event related to S-scale model railroading, please e-mail the information to [dispatch@nasg.org](mailto:dispatch@nasg.org) as soon as possible so that we are able to print it in a timely manner.

**DUE TO THE COVID-19 PANDEMIC PLEASE BE AWARE THAT EVENTS LISTED HERE MAY BE CANCELLED OR RESCHEDULED. CHECK THE INDIVIDUAL WEB SITES FOR INFORMATION.**

## 2021 TRAIN SHOW UPDATES

There will be no **World's Greatest Hobby on Tour** shows in 2021. Promoters say WGH shows will return in 2022.

In the case of **Trainfest**, one of the largest all-scale model railroad shows in the nation, plans are being made for an in-person show in 2021.

Train Show Inc. is planning to have both **Greenberg Train Shows** and **Great Train Shows** resume in 2021, conditions permitting.

### March 20, 2021 — Mt. Hope, Ohio

**Mt. Hope Train & Toy Show** at the Mt. Hope Event Center, 8076 State Route 241. Trains of all gauges, Farm and vintage toys plus die-cast models. NASCAR items. Adults \$5, kids 12 and under free. [www.cjtrains.com](http://www.cjtrains.com)

### March 21, 2021 — Lindenhurst, New York

**Northern Spur Model Train, Diecast Vehicles & Toy Meet.** Firemen's Memorial Park, 555 Heiling Blvd. (north off Hartford St.) 8:30 am to 1 pm. \$5.00 adults, under 16 free w/adult. Contact: Carmelo Sancetta. P.O. Box 1286M. Bay Shore, NY 11706. 631-666-6855

### April 22 through 24, 2021 — Fresno, California

**SUGAR PINE CENTENIAL 2021 PCR/NMRA CONVENTION** at the Wyndham Garden, Fresno Yosemite Airport, 5090 East Clinton Way. See the Registration page for online and mail-in registration forms. **June 10 through 13, 2021 — Greenville, South Carolina**

**SWAMP RABBIT EXPRESS 2021 Southeast Region**

**NMRA Convention** at the Hilton Hotel, 45 West Orchard Park Drive. [www.swamprabbit.org](http://www.swamprabbit.org)

### June 17 through 20 — Tulsa, Oklahoma

**The 2021 TULSA UNION CONVENTION** is a joint production of the Mid-Continent and Lone Star Regions NMRA at the Embassy Suites Hotel, 3332 South 79th East Avenue. The Convention will feature operating sessions, layout tours, clinics, contest room and "non-rail" activities. **July 12 through 18, 2021 — Santa Clara, California**

**RAILS BY THE BAY 2021, NMRA National Convention** at the Santa Clara Marriott Hotel, 2700 Mission College Boulevard (at Great America Parkway), Santa Clara, California.

### September 8 through 11, 2011 — Irvine, California

**ORANGE BLOSSOM SPECIAL — Pacific Southwest Region NMRA 2021 Convention** will take place at The Hilton Irvine/Orange County Airport Hotel. We will offer layout tours, ops sessions, prototype tours, contest room, as well as many clinics postponed from the Van Nuys Local 2020. The Full Fare price of \$125 includes: Welcome Reception, General Membership Meeting (with continental breakfast), Hobo Auction, Swap Meet, and Awards Banquet. More info at [www.PSRConvention.org](http://www.PSRConvention.org)

### August 3 -7, 2021, Buffalo, New York

**National Association of S Gauger National Convention** at the Buffalo Marriot Hotel. The Western New York S Scale Association is the sponsoring club with assistance from members of the S Scale Workshop (Canada). Check the NASG website for updates.

## CLASSIFIED ADS

*ANY ACTIVE MEMBER of NASG can place a non-commercial classified ad in the Dispatch for no charge. Ads must be limited to 10 lines+-. E-mail your ad copy to [dispatch@nasg.org](mailto:dispatch@nasg.org) or mail to Richard Caugherty, Editor, 714 Twin Oak Drive, Pittsburgh, PA 15235. Please be sure to include your member number. Ads will run for three consecutive issues unless told to stop.*

### FOR SALE

**FOR SALE:** GP 20 Diesel Illinois Cnetral Gulf #8001, Birthday 1987, \$150.00 New AF Lionel Erie Berkshire #3360 Birthday 10/15 Flyer Chief control \$250.00, GP7 Diesel Union Pacific #125 Birthday 2018, Flyer Chief Control \$225 I will pay postage and shipping on these three. Also have All ON30 Bachman Spectrum for sale. Call Roger 507-676-0665

**FOR SALE:** Moving to another state. Dismantling AF layout and selling most of collection of S gauge trains, track, switches, scenery, houses, etc. Send email to [fmarotta@frontiernet.net](mailto:fmarotta@frontiernet.net) for my for-sale-list.

**FOR SALE:** S Gauge Herald 1961 to 1988 collection, 106 in all. Not sure if missing items were missed or never issued. Email [brownkc@bellsouth.net](mailto:brownkc@bellsouth.net)

and I will email list of magazines. All issues for sale in one batch—no exceptions.

**FOR SALE:** American Models Pennsy GG1, Brunswick green, five pin stripes, DC, scale wheels and Kaydee couplers, excellent condition, \$175. Three American Models Santa Fe Budd coaches, chrome plated, scale wheels and Kaydee couplers, excellent condition, \$75 each or all three for \$210. email [james.sweeney3@comcast.net](mailto:james.sweeney3@comcast.net)

**FOR SALE:** AF #15 Directonic Rectifier \$25, AF Work Car \$25, AF HO Train #5318 New York Central includes track, caboose, track car, wheel car, engine (does not run) \$20, AF NTTM Work Train: Combine #6-48240 \$65, Gondola #6-48230 \$45, Box Car #6-48234 \$50, Caboose #6-48247 \$60, Flat Car #6-48226 \$42, Tank Car #6-48223 \$39. Buy entire Work Train \$245. Your

address needed to calculate shipping. Robert E. Hartzell, Phone 610-799-2470

### WANTED

**WANTED:** 1964-1972 'S' Gaugian magazines.

Contact by e-mail: [ho.dave1@gmail.com](mailto:ho.dave1@gmail.com)

**WANTED:** Sergent Engineering S Scale Type E coupler kits (Part # EC64K). Lastest investment cast couplers only, Do not need earlier white metal kits. Frank H. Peacock, [fpeacock3006@gmail.com](mailto:fpeacock3006@gmail.com)

**WANTED:** S Scale river Raisin Orange Burro Crane, Putt Trains 4 Wheel Diesel Switcher #55 Blue & #56 Yellow, Putt trains #41 Yellow Shaker Heights Trolley car. Putt Trains #23 North Shore Line Trolley car. SHS #01736 NW-2 B-B (Chinese Red) Diesel Switcher. James Wright 218-780-4691

# *Binge building: the missing section*

Jeff Madden

**A**s Paul Harvey used to say – “Here’s the Rest of the Story.” [Ed. Note: In the January-February issue Jeff had not sent the middle section of his article and neither of us realized what happened until it was published.] It left off after the line on page 3 that ended at a junction spot.

Needing a depot and a tower at a junction spot, I decided on building a Scenery Unlimited resin Reading station kit and doing slight modifications to a Plasticville tower. The station kit went together OK, but I am not a fan of epoxy cement. I pre-painted the station and tower pieces with the Afrika Mustard spray paint and brush painted the trim with Floquil Grimy Black. I shortened the Plasticville tower a bit and added window glass and masking tape shades. See picture on page 3 of the Jan.-Feb. Dispatch.

Next up was the LVM large water tank kit. The supporting framework went together as per instructions, but the tank and roof proved more challenging. The tank framework collapsed when trying to add the wrapper, so I used a solid wood core from the scrap bin to wrap the wrapper around. This worked better until I tackled putting the fishline banding around. This became my “Waterloo” with this kit. With the base finished I went to the scrap bin and rescued a plastic molded tank top with peaked roof that was part of a Lionel O kit. Although slightly smaller in diameter, it worked perfectly.

Being a S scrounger, I had two more of the branch line type “O” Lionel water tanks that had the brick base instead of the typical wood framing. All I did was trim one base short and sink the other a bit into the foam layout base to match the height of S. The common buff color was used on the tanks. An S door in the bases and S ladders completed the task.

During the cold weather days of 2019 and 2020 more projects hit the workbench. Next was LVM Hawk Milling Co kit. With previous LVM experience things went well, and pre-painting was key.

After Christmas 2020 I decided to tackle two “simple” laser wood craftsman kits: the Lake Jct. B&O freight house, and the

Monroe Models’ B&O Pump House. Simple, well kind of! There were some issues that required trimming and forget those peel and stick shingles. Instead, I used masking tape painted black to resemble tarpaper. Again, the buff paint was applied to the siding with the trim brush painted black.

Encouraged, along came two more small laser kits – an Allegheny Models octagonal crossing shanty and a Laser Art flag stop depot. The crossing shanty was the most tedious because of all the windows needed. The B&O paint scheme and masking tape roofs were used on both.

Next up was a Twin Whistle gas station. Not a bad kit, but the instructions left a bit to be desired (maybe they have improved by now) such as figuring out which pieces were pre-cut, and which had to be cut to length. On my own I added ¼” square support beams for roof support.

Now for my first B.T.S. laser kit, the rural church – oops, I was not fearful but due to layout space I decided instead to upgrade a plain plastic (GloLine I think) church by adding Evergreen wood siding to it and painting. I was satisfied so the B.T.S. church goes to the sale table. But do not despair as a B.T.S. 2-story house was not to be abandoned, and this turned out to be the best laser kit I had built so far. The only modifications I made were substituting Grandt Line 4-pane plastic windows and Grandt Line S doors for those “pain-in-the-neck” fragile laser ones.

As March 2020 arrived it was back to the workbench. A second Twin Whistle kit with mostly styrene parts, pulled off the shelf was the A&W Root Beer stand. Instructions again were lacking. The only color picture was on the cover of the box, and 3-D drawings were still absent. Again though, I managed with guess and by golly to get things correct. I used an orange Rustoleum spray can, and the decals were particularly good.

And since I was still trapped in the house as March ended, I spied another unbuilt kit – the Des Plaines resin ‘50s Gulf Gas Station. It was a nice kit to build but needed wall stiffening with styrene. I did shatter one garage door trying to remove flashing, so I have one door down and one up. Overall, the kit went together well with

the usual pre-painting good decals.

Also, I built a Mullet River 2-stall out-house kit while doing some finishing touches on an East-West 2-bay gravel trestle. Here I substituted a scrap HO bridge deck and some scrap pre-formed railings for the laser parts. I love shortcuts.

Another LVM kit was soon pulled off the shelf April began, and this was the Sand House kit. Yes, it went together well, but I used some more shortcuts where I substituted a more stable platform with some scraps left over from a Plasticville coaling tower, and for the spout mechanism I used plastic scraps and hollow electric wire to fashion my own.

The Twin Whistle Diner was next although the TW instruction sheets lacked sequence steps and 3-dimensional drawings. I did my own thing for a few steps. I found scrap plastic steps and railings for the end platform and shortened an arched wood passenger car roof which fit perfectly over the built-up sides and is easily removed to see the interior details. Also, I used some scrap styrene pieces for the interior counter and stove and two push pins for two stools.

Back to modifying a Plasticville building. I took the Mansion House and painted the roof black chimneys red, added interior clear plastic in the windows, added paper curtains from old HO kits and added signage to create a small hotel.

Other things accomplished on my layout were adding signage to structures already in place. Now comes the messy part – finishing hills, waterways, roadways and backdrops.

Looking back on this structure binge building, I know that I prefer styrene kits, even kitbashing styrene kits, over wood kits. For wood kits I prefer Grandt Line (now San Juan) plastic molded doors and windows to be included in almost all laser kits. Also, I prefer detailed step-by-step instructions with 3-D drawings and color photos of a finished model. If kits are of a prototypical building a mention of paint schemes would be helpful.

**Note: Check the NASG website for current structure company websites, many of which were mentioned in this article.**

# ANNUAL NASG CAR OFFERING

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Jeff Madden, New Products Editor

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are offered on the GP35 and the Baldwin S-12. Prices start at \$249.95.

**DES PLAINES HOBBIES** ([www.des-plaineshobbies.com](http://www.des-plaineshobbies.com)): Now has blank, undecorated, 40' PRS boxcar kits available. No couplers or trucks. Comes with instructions. \$40 ea.

**DIECAST DIRECT** ([www.diecastdirect.com](http://www.diecastdirect.com)): One of the newest 1/64 items shown is a modern Chevy C65 tandem



axle grain truck for \$64.95. Like 3000 Toys (above) they list many diecast vehicles in 1/64.

**FIRST PERSON SCALE MODELS** ([www.shapeways.com](http://www.shapeways.com)): This outfit appears to be part of Shapeways. On our own NASG website there is shown a W&OD 70 series interurban body that

is 3-D printed. Also, on Shapeways web under First Person a whole gamut



of odd diesel type and doodlebugs are shown. If available, they would be without chassis and power.

## GREAT DECALS

([www.greatdecals.com](http://www.greatdecals.com)): Bill Mosteller now has some 1945-'69 gold (yellow), and black diesel nose decal sets that come along with road names and numbers as well. These are \$6.95 plus



shipping.



**LARKSPUR LASER ART** (Phone – 613-269-3690 -Canada):

Has a laser kit of the Enfield Powder Co. brick storage building for \$89.99. It measures about 5 inches square.

**MOTRAK MODELS** ([www.motrakmodelsusa.com](http://www.motrakmodelsusa.com)): Has a retail paint rack for 1.35 oz. bottles for \$35. It measures 10-3/4" x 12-3/4" x 2" deep.

**OUTLAND MODELS** ([www.outland-models.com](http://www.outland-models.com)): In 1/64 scale they show



several modern city structures including garages, stores, and office buildings. I assume plastic. Prices range from \$22 to \$35. They also show 1/64 figure sets that range in prices from \$17 and up.



## PRE-SIZE

([www.pre-size.com](http://www.pre-size.com)): Newly shown is 1987 Greenville gondola coil covers. This kit

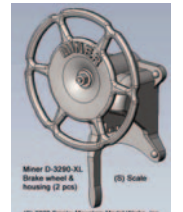


is designed to fit the Greenville gondola. \$38.50, includes shipping.

## SMOKY MOUNTAIN MODELWORKS

([www.smokymountainmodelworks.com](http://www.smokymountainmodelworks.com)): Newly shown are two

brake wheel assemblies – one is a Miner, and one is an Equipco. Coming also are many door parts.



## Russell M. Mobley Memorial Library Magazine Resources

*3/16 "S"cale Railroading*  
*Essence - the NMRA "S" SIG*  
*newsletter, 1985-1988*  
*Mainline Modeler 1980 - 1997*  
*Model Railroader 1937 - 1961*  
*NASG Dispatch • S Gauge Herald*  
*S Gaugian • S/Sn3 Modeling Guide*  
*Sn3 Modeler*

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# CLUB CARS FOR SALE

**ANY MEMBER CLUB** with special edition cars from conventions or fests can advertise them here for no charge. Send a hi-res jpeg photo and information to [dispatch@nasg.org](mailto:dispatch@nasg.org)

**North Florida Div. of Atlantic Coast S Gaugers** is offering Lionel AF boxcar in Florida's colors to support North Florida ACSG. \$100 SH included or \$80 @ a train show. Check to Larry Shughart, 5216 Lourcey Road, Jacksonville, FL 32257



**2019 NASG CONVENTION BOX CAR** featured the US Playing Card Co. Bicycle logo. The white bodies/blue roofs display the red logo #1985 & red roof displays the blue logo #2019. Hi-rail & scale available for \$70 plus shipping. Visit [www.COSG.Club](http://www.COSG.Club) for info and order form or call COSG 614-766-9033



**S FEST 2018 ILLINOIS CENTRAL REEFERS AND GONDOLAS**



CASG offers yellow and orange reefers for \$57 yellow & \$75 orange (only 32 produced) each in 2 different Specify hi-rail or scale. Contact Leigh

road #. Shipping \$7.95 per car.

Maginniss 847-401-8672 for availability. To order specify cars desired, mail check payable to CASG, to Leigh Maginniss, 314 S. Drury Lane, Arlington Heights, IL 60004

**SOHIO TANK CAR** These Lionel American Flyer cars sell for \$70 including shipping. Make checks payable to SCSGC, mail to Jeff Kruger, 1865 Nottingham Lane, San Dimas, CA 91773-3718



**BURLINGTON REEFERS** State TRACK & line S-Gaugers have reefers for \$65 including shipping. To order make checks payable to SLSSG, mail to Tom Behles, 15804 O'Brien Rd., Harvard, IL 60033



**2017 CONVENTION CARS AND TRAILERS TOFC \$60 each, TRAILERS \$20 each**



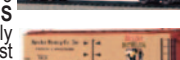
Shipping: \$11 1st item, \$4 each additional item. **BALTIMORE AREA AF CLUB** has TOFC & trailers B&O #2017 in scale, B&O #9523 in hi-rail & scale, WM #1889& #2411 in scale & hi-rail. Extra trailers available. <http://baltimoreamericanflyerclub.org/wp-content/uploads/2017-CarForm5R-1.pdf> Order from BAAFC, c/o Kyle Russell, 4146 U Way, Harve de Grace, MD 21078



**FEAST SPRECHER REEFERS** has a Deep Rock tank 2 car set for \$159.95 + \$15 shipping. Checks payable to GGAFCC, send to Paul Guaraglia, P.O. Box 4054, Foster City, CA 94404-



**2012 S FEST SOO LINE CONDOLAS** in limited supply with loads @ \$40 ea. + shipping \$10 first car, each add. \$4. Loads avail. in 3 colors Hi-rail, or Scale. #2012 or 67491 **2016 S FEST SPRECHER REEFERS** limited supply \$55 for Hi-rail, \$60 for Scale. Shipping \$10 first car and Each add. \$4. # 2016 or 20025. Call 414-421-8512 or [rons48tblue@gmail.com](mailto:rons48tblue@gmail.com)



# COMPANY STORE

FOR SALE TO THE GENERAL PUBLIC

**NMRA S3 TRACK AND WHEEL GAUGE**  
\$7 members

**NASG S-3, S-4 TRACK & WHEEL GAUGE** \$7 and 2 for \$12

**#802 and #5 KADEE COUPLER HEIGHT GAUGE** Measures car on track proper 17/32" height. \$6 each

**REFRIGERATOR MAGNET**  
50TH ANNIVERSARY LOGO \$4 each

**CLASSIC ERA S SCALE CLEARANCE GAUGE**  
\$7 members  
\$9 non-members

**MODERN ERA S SCALE CLEARANCE GAUGE**  
\$7 members  
\$9 non-members

FOR SALE TO NASG MEMBERS ONLY

**NASG T-SHIRT**  
\$15  
2X \$18  
ash gray with blue logo

**NASG LAPEL PIN**  
3/4 diameter  
\$5

**NASG CAP**  
\$19  
Tan & blue denim with color logo

**NASG GOLF SHIRT**  
\$29  
2X \$33 dark blue w/pocket yellow logo

**EMBROIDERED PATCH**  
\$4  
2 3/8" diameter

TO ORDER any of these products, download an order form at [www.nasg.org/Store](http://www.nasg.org/Store) or write your order legibly on a piece of paper. Make checks payable to NASG Company Store and mail to: Shane Lambert, 2091 Vining Drive, Unit D, Woodbury, MN 55125

**IMPORTANT:** Be sure to include your member number, quantity desired and sizes. **PRICES include shipping to anywhere in the continental US.** All shipping is by US Mail. For shipping outside the US contact the Company Store. **Wisconsin residents** please include appropriate sales tax and county of residence. If you have questions contact Storekeeper Shane Lambert 651-689-3206 or e-mail to [companystore@nasg.org](mailto:companystore@nasg.org)  
**TO SHOP ONLINE:** Go to <http://www.nasg.org/Store/CompanyStore.php>  
Choose the items that you want to purchase and click Add to Cart and follow the instructions.

## 2019 American Flyer Commemorative Car \$69.00

includes domestic shipping. Since 1981 NASG has contracted with Lionel to produce a limited-run car so a caboose represents the end of the line. **NOT PART OF SALE**



**2017 and 2018 American Flyer Commemorative Cars \$69.00 each or two for \$130.00**



**2016 American Flyer Commemorative Car \$69.00**

A CNW flatcar loaded with produce trailers. \$69.00. **Car \$69.00** Lionel BNSF waffle sided boxcar



**2015 Convention Car \$68.00**

Kansas City Southern



**2006 General Electric Crane Car \$70.00**  
**2007 General Electric Boom Car \$52.00**  
**BOTH CARS ONLY \$115.00**

## NASG CAR DISCOUNT SALE

**ALL CARS ON THIS PAGE EXCEPT THE 2019 CABOOSE ARE PRICED AS FOLLOWS**  
Buy five or more cars — Take 30% off  
Buy four cars — Take 25% off  
Buy three cars — Take 20% off  
Buy two cars — Take 15% off  
Buy one car — Take 10% off

Please check the NASG website for the most up to date information on car availability since some may sell out by the time The Dispatch is printed.

**2010 Jenney Gasoline Triple Dome Tank Car \$75.00** by Lionel road number 2910



Sales of cars temporarily suspended

## TO ORDER THESE CARS

Make payment out to NASG and mail to:

Dave Blum, 3315 Timberfield Lane, Pikesville, MD 21208

Please include the following in your correspondence:

1. Your NASG ID number from your membership card.
2. Your name and shipping address.
3. Which car or cars that you want and how many of each.
4. Your telephone number or e-mail address in case there are any questions about your order.

5. Maryland residents add 6% sales tax to your order.

**All prices include shipping to addresses in the United States.** Canadian orders please add \$10. To ship outside the continental US or for any other question contact Dave Blum at [pikesville-models@yahoo.com](mailto:pikesville-models@yahoo.com)

**TO SHOP ONLINE** Go to [www.nasg.org/Store/AFCars.php](http://www.nasg.org/Store/AFCars.php) for the AF cars or [www.nasg.org/Store/ConvCars.php](http://www.nasg.org/Store/ConvCars.php) for Convention Cars and click on the **Add to Cart** button for the item(s) that you want to buy and follow the instructions.

## New items 2020

[www.americanmodels.com](http://www.americanmodels.com)



Milwaukee Road GP9



Milwaukee Road S12



Lehigh Valley S12



Lehigh Valley RS3



Burlington Northern Santa Fe.. GP-35



CSX GP-35



Toledo Peoria & Western  
3 bay coal Hopper.



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