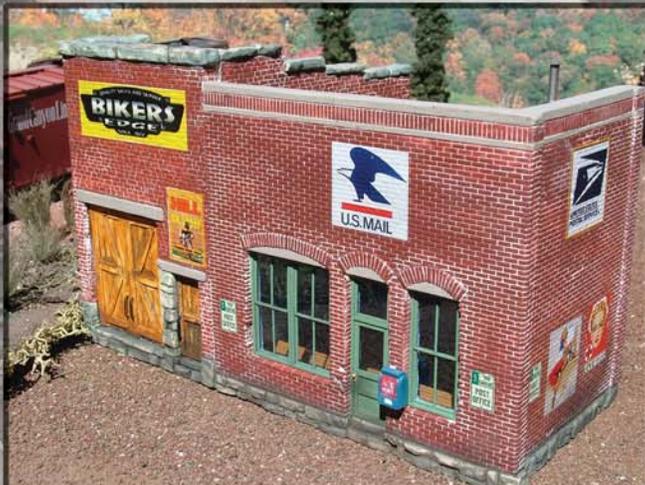


DISPATCH



**2009 Convention Update
Reneris Hi-Rail Layout
NASG Convention Car**



McCabe Lumber Co. Planing Shed

The McCabe Lumber Co. Planing Shed should be called a planing mill complex since it is more than just a simple shed. The planing complex consists of the transfer shed, two planing buildings, boiler house, water tank, and loads of details, and is a key part of the Slatyfork Sawmill Complex. The Planing Shed can also be used as a great stand-alone manufacturing industry such as a box factory.

This kit consists of laser-cut basswood, plywood, and cardstock, tarpaper roofing, brass, urethane, and white-metal detail castings, and loads of character. The tabbed, well-engineered construction provides fast and easy assembly. The footprint is about 70' x 90'. **#08225** **\$349.95**



HO Models Shown. Detail castings vary between scales.

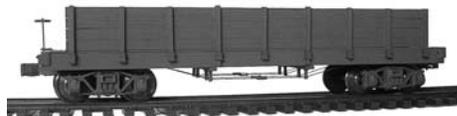
American Flag Company & Slatyfork Sawmill Coming Next!
Check our web site or call for info.



34' Truss Rod Flatcar

This flatcar was inspired by an early drawing for one that ran on the V&T. This was before the time of air brakes, and the car featured link & pin couplers when built. It is a very generic car that can be used on many lines. The kit consists of laser-cut wood and styrene details. Less trucks and couplers.

#09603 **\$ 25.95**



34' Truss Rod Gondola

The gondola is an offshoot of our 34' flatcar with laser-cut sides and end boards. Two optional end sill steps are included. The kit consists of laser-cut wood and styrene details. Less trucks and couplers.

#09604 **\$ 39.95**



PRR H21 Hopper

Starting in 1911, 70-ton H21a "quad" hoppers were introduced for coal service. The kit consists of high-quality, no-odor urethane castings for the body and brass brake components. Less decals, trucks and couplers.

#09204 **\$ 79.95**

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NASG DISPATCH
Official Publication of the
National Association of S Gaugers

The NASG DISPATCH welcomes art, photographs, letters, articles and other S gauge/scale related materials contributed by the membership. Send all such materials to the editor.

Jeff Madden
438 Bron Derw Ct.
Wales, WI 53183
262-968-3729
nasgdispatch@hotmail.com

Membership year is determined by the date you sign up guaranteeing 6 issues starting with the next issue after signup. All applications, renewals and membership questions should be directed to:

NASG Treasurer,
Michael Shea, 488 Stonewood Ave.,
Rochester, NY 14616 -3623
Phone: 585-865-4978
email: mikeshea@frontiernet.net

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The **Dispatch** - ISSN 10457178 is published bimonthly by
 Studio G
 PO Box 745
 Jonestown, PA 17038-0745
Email: T.Chuck Garman at
 dispatch@tchuck.com

SUBSCRIPTION RATES: 1 year, \$20.00 which includes membership in the NASG. All subscriptions payable in U.S. funds. Postage is paid at Harrisburg, PA. Printed in the USA. All rights reserved.

Postmaster: Send address change to
Michael Shea
488 Stonewood Ave.
Rochester, NY 14616 -3623

MAILING: The Dispatch is printed in the issue month and is mailed by the 4th Friday of that month. Please allow 2 weeks for delivery.

COVER: George Reneris surveys his hi-rail layout featured in this issue. Gerry Evans builds a brick post office. The Lemp Falstaff car is one of two Falstaff cars being offered as the NASG convention car by the St. Louis sponsors.

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February 2009



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NASG Website: <http://www.nasg.org>
NASG Website Director Michael Greene: mgreene@cedarlane.com

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The DEADLINES for articles are the 1st of February, April, June, August, October, December, for issues dated April, June, August, October, December, February, respectively.

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WELCOME TO THE EAST TENNESSEE DIVISION,
 The newest division of the Atlantic Coast S Gaugers. At a formational meeting on January 31st at the Southeastern Regional Train Center in Johnson City, TN, 16 prospective members showed up from NC and as far away as Nashville, TN. The meeting was hosted by Gary Cameron, coordinator for the new Division and led by Larry Moore of the Carolinas Division. Larry provided handouts and led the group through the specifications of module building. Bob Lacheen brought a straight module from the Georgia Division, Marvin Thiel brought one of the Carolinas Division's T modules, and Joe Haenn brought a straight module with a siding. The group decided to move forward and build modules in anticipation of having an operating modular layout at their Fall Railroad Days Train Show (as it was not possible to get a basic layout completed by their April 4th Spring Railroad Days Train Show).

This Division will serve Tennessee from Nashville east, western North Carolina, southwestern Virginia, and southeastern Kentucky. Contact Person is:

Gary Cameron
 110 West Market Street, Johnson City, TN 37604
 423/833-3383
 setraincenter@gmail.com
 garyjcameron@wmconnect.com

Following the meeting, participants toured the Southeastern Regional Train Center, which currently houses large S gauge and O gauge layouts, a 6 by 12 panel American Flyer Pikemaster layout featuring 6 winter scene panels, an On30 layout, a large ceramic village layout, and small HO and N scale layouts. Following lunch, the group visited the Toy Train Museum and large HO and N scale layouts located on the campus of Eastern Tennessee University.

Johnston City, Tennessee was established and grew up because of railroads. It was once the home to 3 major railroads and terminals--Cinchfield, Southern, and Eastern Tennessee and Western North Carolina (Tweet-sie). Today Johnson City is served by the CSX and Eastern Tennessee railroads. -Joe Haenn



S HELPER SERVICES NW2 PRESS RELEASE:

Shortly before its purchase by General Motors, Electro-Motive Corporation introduced its second generation diesel-electric switching locomotive, the NW-2. Responding to the railroad's requirements for more powerful switching locomotives, the NW-2's design improvements incorporated a larger, 1000HP engine, a completely welded frame, and changes in body styling. Built from 1939 until the end of 1949, NW-2s proved to be EMD's most popular switcher, and enjoyed the longest production run of any EMD product. Almost 1,150 units were delivered to railroads in the U.S. and Canada.

Second run Road Names now available include ATSF, CB&Q, PRR, and unlettered black. The NW-2s are available with LocoMatic™ sound or DCC right from the factory! Other features include "golden white" LED headlights and interior cab lights. Check the on-line ordering page for a list of all the roadnames!



Right out of the box, your NW-2 will run on any layout with American Flyer® compatible track. We also include all the parts necessary to operate with code 110 RP-25 wheels. For details log on to:
www.showcaseline.com/indexNW2.html

NASG CONVENTION CARS

The convention cars for the 2009 Annual Convention to be held in St. Louis in July (see New Product Report, page 6).



'S' RAIL MAIL

S Event Lodging Dilemma: Having suitable hotels is probably easier with a smaller convention like the NASG rather than some of the larger ones like the National Narrow Gauge Convention or the NMRA National. Look at some of the recent hotel rates for the NMRA Nationals, and you'll see that the NASG convention committees generally do a good job of trying to keep the rates reasonable. Actually, the tours seem to be the most expensive part, but those are optional. In general, I wish the layout tours were self-

guided – pick your own – rather than bus tours.
- Dave Heine

More Event Lodging: The only problem with staying “off campus” is that most conventions need large meeting rooms, banquet rooms and trade show area. These are leased out based on the number of folks staying at the hotel. So if everybody stayed at the local Super 8, you will find the cost of the convention going up to cover the cost of those lost room rentals. When our local division of the NMRA sponsored the Lone Star Convention this past summer, people who stayed at non-convention accommodations were billed an additional fee to cover the convention facilities. I'm not sure

Continued on page 30

Jeff's Junction



The Expense of Attending S Events – or How to Afford to Go!

Yes, conventions, sprees, fests, cost you dollars besides what you might want to spend on S goodies. It's like any family getaway, vacation or fishing weekend – you have to budget for food, travel and lodging expenses. And yes, you should try to attend some of these events if you really want to enjoy our minority scale. Why you say:

1. Fellowship: Attending all-S events allows you to meet other S folks in person when you might have only seen or heard of them via magazines and the internet. Here you'll have face time with other S modelers who may have solutions to your questions or you may have answers to theirs.

2. How-To Clinics: In-person clinics give you a chance to be educated about most any S specific or model railroad subject from building Styrofoam mountains, to repairing Flyer locomotives, to DCC, to weathering freight cars. Sure, you can view DVDs or read magazine articles, or interact on the internet, but believe me folks, it just ain't the same as being there.

3. Contests: Here you can show off your best models in contests or at least view in person other modelers' handiwork in S.

4. Layout Tours: Seeing a display layout or home layout firsthand just can't compare to articles or videos.

5. Buying and Selling: At an all-S event items for sale are all S. There is no place else to do this in person. Here you can feel, touch, inspect and examine items and talk or haggle with the vendors. On the flip side, you can be the vendor yourself and help pay expenses that way. Note that almost all S events I have attended have very reasonable table costs –

it's always the same for manufacturers, retailers and the plain old modeler. No \$300s plus a booth here! And, 90% of the items are 1/64 – no endless searching out for that elusive table full of S items that you'd have to do at an all-gauge event. You don't have to wear yourself out wandering the country miles such as you find at York or other big meets.

OK – so here are the reasons to attend. I've whetted your appetite. Now, let's examine how you attend on a reasonable budget. Plus, the following tips are extremely appropriate to today's lousy economy filled with layoffs, high gas prices, and debilitated savings. The truth of the matter is though – “where there's a will, there's a way.”

1. It's a Vacation Stupid. If you have a reluctant spouse or limited vacation time from work, then plan the NASG convention or S event around a vacation, even if you have to attend for only a portion of the time. For a reluctant spouse, hype up the non-rail tours, the hotel, the banquet. Stop at the S event on the way to or back from a vacation destination. Sometimes the convention or event location can be the vacation destination itself. For example, there are lots of non-rail attractions in the St. Louis area where the NASG convention is this summer.

2. Save on Lodging: If you're with your spouse, you're stuck with paying for two, but convention hotel rates are usually less than the going rate and offer the convenience of being in the area where the activities are. If you're going with the guys, sharing a room cuts the expenses in half or less. If you absolutely must, you can stay in a less expensive motel or even drive an RV down and camp. Of course, the greatest lodging bargain of all is to have relatives or friends living in the area you can sponge off of. I have put up a few friends at my house several times, for example.

3. Car Pooling: If you're going with the guys and sharing rooms, you probably also will car pool and save that way. Those coming by Amtrak or air can also arrange to share a rental car. Those coming from a long distance can search for bargain air fares, and

Continued on page 31

NEW PRODUCTS REPORT

By Jeff Madden

ATLAS (INDUSTRIAL RAIL) (www.atlasO.com). Here's a comment on the PRR E-6 Atlantic mentioned in the April 2008 *Dispatch* from Gene Cimino. "Yes, both the engine and tender on the Atlas Atlantic are within about a scale inch of the prototype. The rods and valve gear are a bit cheesy, but not as toy-like as on the cheaper Flyer Atlantic. The tender is the real beauty and could probably be used for a variety of locomotives for that time period. I'm waiting to get a couple more of these engines when they start showing up at the train shows.

JOHN HALL DECALS (170 Nottingham Road, Nottingham, PA 19362). John has some S scale Reading caboose decals available. Each set contains enough data to letter two Reading cabooses for the time period from 1924-1957. This is John's decal set #110 and sells for \$4.00 per set. Each set contains lettering illustrations and a list of classes, built dates and the number series for each. See photo on page 7. Provide large LSSAE with each order.

KALMBACH (www.modelrail-roaderbooks.com) has a neat book of layouts called *102 Realistic Track Plans* that should be useful to S scalers or hi-railers. Most of the plans are listed as N, HO or O, but there's a couple of Sn3 ones. Regardless it should be easy to convert any of the plans for standard S scale or Sn3. No, this is not the old *101 Track Plans* with a new cover. These are all plans that have appeared to MR publications over the last 10 or 20 years. The softcover publication lists for \$7.95. There are several other softcover publications from Kalmbach titled *How to Build Realistic Layouts* that complement the track plan book. These deal with yards, trackside struc-

tures, industries and city scenery. These all offer lots of creative ideas no matter what scale you model in.

MICRO-MARK (www.micro-mark.com). For those looking for an inexpensive coaling tower, the Plasticville "O" one is a useful starter. Now, you can get a "topper" kit to give the Plasticville coaling tower a more realistic look. This is listed under O scale in the Micro-Mark catalog, but we all know that most Plasticville is undersized. The "topper" kit consists of laser-cut wood parts for a new upper story, a gate and details for the chute, a new back door, corrugated roofing material and laser-cut windows. List price is \$24.95 (not sure who the actual producer is), but Micro-Mark has a \$19.95 price.

A little editorial here - The overall size of the coaling tower's main structure is really pretty right for S. Only the supporting trestle parts are a bit too high. Many might want to create their own topper out of styrene. As to the supporting structure, I would shorten it an inch or so to give it a better height for S. Since the original Plasticville supports represent steel I beams, you might consider filling in the hollows with thinner styrene strips to give the supports a wood look. This type of coaling tower would have been all wood. -Ed

MTH RAILKING (www.mth-trains.com). I'm always checking out RailKing structures and accessories for use in S. Every year they seem to add a few buildings in the \$40 to \$60 range that would work for S as is or with some fairly easy modifications. We're all familiar with the farmhouses, but my eagle eye sees many others that might be convertible: engine house, grainery, freight transfer warehouse, several passenger stations, the Hellgate

bridge, towers, stores, small brewery, etc. Try to inspect these in person at shows or hobby shops and use your scale rule.

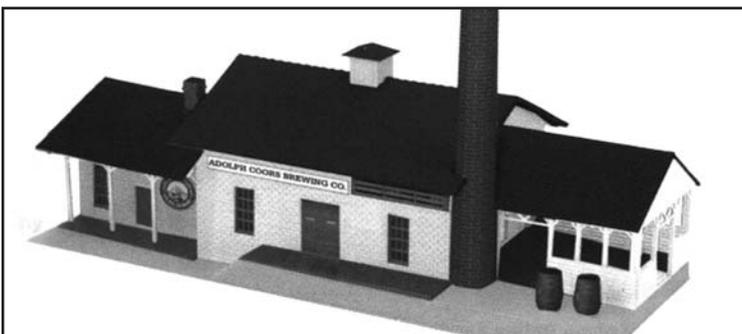
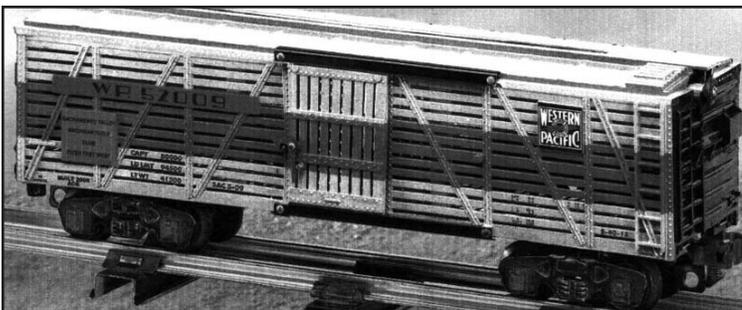
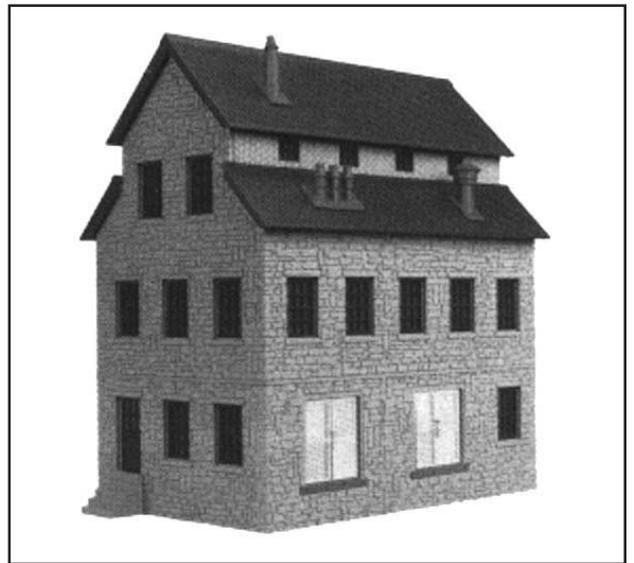
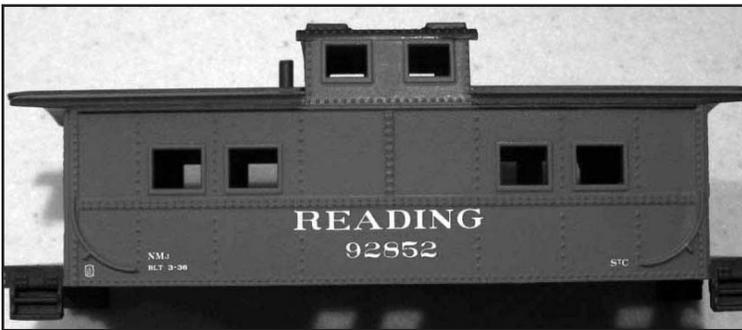
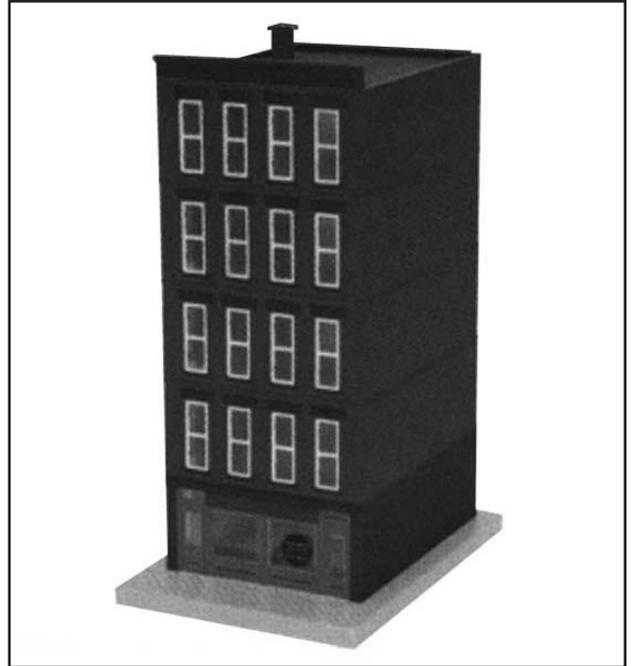
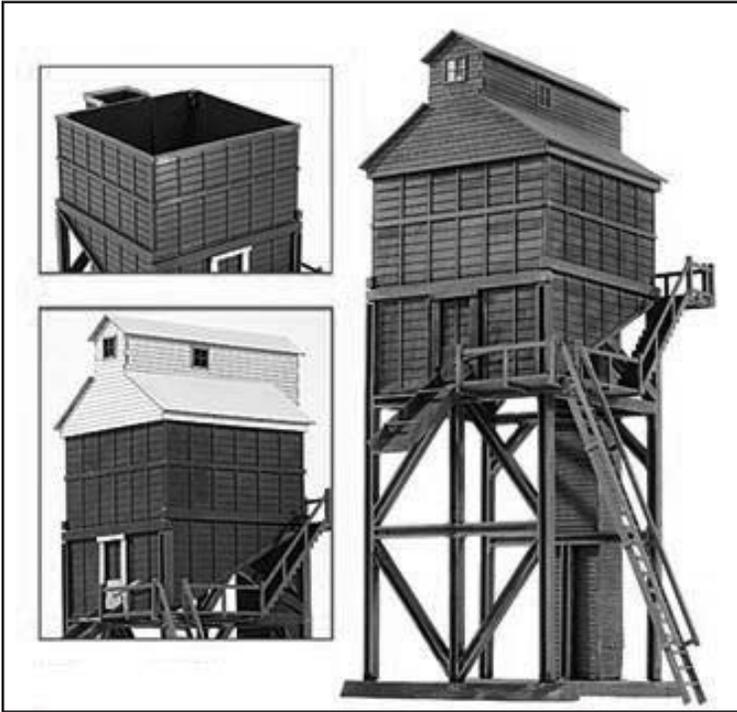
NASG CONVENTION CARS

The convention cars for the 2009 Annual Convention to be held in St. Louis in July are two reefers of a famous St. Louis beer, no not Budweiser, but a fallen flag - Falstaff. The two wood reefers (see bottom page 4) are being produced by S Helper Service. There will only be a limited 400 of these cars produced in 4 numbers each. The Falstaff logo car is orange with oxide red roof and ends and multi-color logo. The LEMP (Falstaff car is red with black roof and ends and yellow and white lettering. By the 1960s Falstaff was the 3rd largest brewer in America, but like many breweries mergers and acquisitions in the 1970s put the brand in decline and it was discontinued in 2005. The limited run cars are \$50 each. Check the website for ordering and S&H charges: www.nasg2009.com

SACRAMENTO VALLEY AMERICAN FLYER CLUB is sponsoring an AF Western Pacific stock car by Lionel for the Flyer Fest West 2009. The car is a colorful silver and orange stock car with black ends. The car is available only to Flyer Fest West 2009 registrants and members. Cars are \$60 plus \$10 registration for the Fest. For details and shipping costs email FFW2009@comcast.net or call 916-682-6961. You can access it on the web by typing in the A.C. Gilbert Heritage Society.



WHAT'S NEW



**MATCH'EM
UP!
MATCH'EM**



Where Rivers and Railroads Met NASG 2009

St. Louis Airport Marriott
August 4-8 2009

The S Gaugers of the St. Louis area invite all S Gaugers to St. Louis for the National Association of S Gaugers 2009 Convention.

Why St. Louis?

St. Louis was the destination for many railroads in the 1800s. Trains brought people and goods to East St. Louis where they were ferried across the Mississippi River to St. Louis. From there other railroads carried the people and goods westward or boarded a steamboat to go up river or up the Missouri River westward.

Which railroads came to St. Louis?

The Southern Railway, Louisville & Nashville Railroad, the Pennsylvania Railroad, the New York Central Railroad, the Baltimore & Ohio Railroad, the Wabash Railroad, Chicago & Eastern Illinois Railroad, Chicago, Rock Island & Pacific Railroad, the Missouri Pacific Railway, the Alton Railroad, and the Gulf, Mobile & Ohio Railroad were some of the railroads which came to East St. Louis and some even crossed the Mississippi. When James B. Eads proposed and then built the first railroad bridge across the Mississippi in 1874, rail traffic started flowing across the bridge instead of being ferried across the river.

In 1889 financier Jay Gould, who controlled six proprietary railroad in the St. Louis, orchestrated the formation of the Terminal Railroad Association. The original railroads were the Missouri Pacific Railway, the St. Louis, Iron Mountain and Southern Railway, the Wabash Railroad, the Ohio and Mississippi Railway, the Louisville and Nashville Railroad, and the Cleveland, Cincinnati, Chicago and St. Louis Railway companies. Subsequently other railway companies joined the Terminal.

Today St. Louis is still a hub of rail activity though consolidation has changed the composition of the Terminal. Terminal operates two bridges across the Mississippi and a switching yard in Madison, IL with 80 tracks to hold up to 2200 cars at one time. Burlington Northern-Santa Fe operates a large yard along side I-44 stretching from Kingshighway to the city limits. Union Pacific operates a smaller yard on the southern edge of downtown as well as a larger yard in Dupon, IL south of East St. Louis.

Activities Planned

For the convention we are offering some unique opportunities to see the area. We are including as a part of your registration, an opportunity to visit the Museum of Transport in St. Louis County. Also on Friday, we will have a tour of the Union Pacific Car Repair Shops in DeSoto, MO with a luncheon at the Hotel Arlington. The Arlington is now a B&B but has a railroad motif room where we will eat. It was a railroad oriented hotel. We will have a live steam (12" gauge) ride along the Meramec River on a three mile route behind one of the 11 steam locomotives of the Wabash, Frisco & Pacific Railroad in Glencoe, MO. For diversion, we will travel out along the Katy (Missouri, Kansas & Texas RR right-of-way) Trail to the oldest wine producing region of Missouri. We will stop at 3 wineries and sample the goods, and have lunch at one of the wineries.

For the ladies we will offer a trip back in time to the village of Kimmswick along the Mississippi with luncheon at the Blue Owl restaurant and an opportunity to shop at the various antique and craft shops in the village.

Of course, no trips to St. Louis would be complete without a visit to the Anheuser-Busch Brewery in St. Louis. The tour of the brewery will be followed up with a tour of the facilities of the Manufacturers Railway, which is the rail transportation arm of A-B. Sampling will be optional.

Additional Updates on the News/Contacts page on the website: www.nasg2009.com

Hotel

The Airport Marriott room rate is \$99 for the convention and for three days before and after the convention days. There are indoor and outdoor pools at the hotel.

Railroad Related Tours

- Union Pacific car repair shops in Desoto, MO.
- Anheuser-Busch Brewery tour including Manufacturers Railway System shops and yard.
- MetroLink Light Rail shops and operations center
- Short Line Railroad Yard tour.
- Museum of Transport
- Wabash, Frisco & Pacific 12" gauge railroad.

Non-Rail Tours

- Kimmswick, Missouri river town with lots of antique stores, gift shops and restaurants.
- The Mall at Union Station Tour via MetroLink
- Missouri winery tour with lunch.

Other area attractions (self-guided)

- The Arch
- St. Louis Zoo (yes they have a train)
- St. Louis Art Museum
- Six Flags Amusement park nearby
- St. Louis History Museum
- Ted Drewes' Custard stand on old Route 66

Layout Tours

There will be four layout tours during the week.

Regular Convention Activities.

There will be the usual activities at the convention hotel including many clinics, sales hall, displays, contest, banquet and auction.

Convention Hotel

The convention hotel is the Marriott Airport, naturally, near the airport and across the street from the MetroLink. The direct line to the hotel is 314-423-9700. Ask for the group NASG2009 to get the special convention rate. If online it's www.marriott.com - use code nasnasa.

Getting to St. Louis:

- **Airplane, of course:** The airport is practically within walking distance of the convention hotel, and there is a free shuttle.

- **Amtrak:** Well, there's the Texas Eagle if you're coming up from Texas or Arkansas. If coming from California, you can take the Sunset to San Antonio and transfer to the Texas Eagle. Or, the Southwest Chief will get you to KC where you can transfer to a KC-St. Louis Train. Otherwise you can take many different trains to Chicago and take a 5 hour hop to St. Louis from there. And once you get to the St. Louis Amtrak station (next to the original one which is now a hotel and mall), you can take Metrolink almost directly to the Marriott Airport.



- **Highway:** *"Get Your Kicks on Route 66."* St. Louis is in the center of the midwest, so driving there shouldn't be overbearing unless you're coming from the far west coast. And since many of us are nostalgia nuts, there is an added bonus in you want to explore a bit of **old Rt. 66** which is still traceable across Illinois from Chicago and west to LA. If you come through Illinois from the north or east it basically parallels I-55.



Pontiac, Bloomington, Lincoln, Springfield and Litchfield are some of the more popular "old road" spots you might want to investigate. If you're coming from the west or southwest, Route 66 parallels I-40 to Oklahoma City and then I-44 to St. Louis. As a bonus, many main railroads follow along nearby.

For those just bee-lining it to St. Louis, Route 66 still features some nostalgic attractions in the immediate metro area. The old "Chain of Rocks" bridge across the Mississippi between Illinois and Missouri is now a walking trail if you are so inclined - or Jamie, I think you could take your bike across. Manchester Rd. out toward western St. Louis still is a mecca for those wanting a custard instead of ice cream. Ted Drewes' Custard Stand is world famous right on old Route 66. Just west of St. Louis you can parallel I-44 out to Rolla, MO and take in such attractions as the Jesse James Wax Museum, Meramec Caverns, Onondaga Cave, and a bunch of wineries around Rolla. The BNSF (Frisco) parallels both I-44 and old Rt. 66 pretty much all the way to Oklahoma City. The UP (ex Missouri Pacific) parallels only about 10 miles or so out to Pacific. If you are intrigued about Rt. 66, you can just Google the internet to find out more about it.

So cruisers and railfans should be in heaven in the St. Louis area or getting to and from. I'll cover some dining info and railfan spots around St. Louis proper in the next issue or two. Not much in the way of classic diners out here - but lots of BBQ, Steak 'n Shakes and so on to please most any palette.

Also, if you're driving there are some neat on-the-way spots you might want to check out - Galesburg, IL (railfan heaven), Springfield, IL (Lincoln stuff, Abraham that is), Hannibal, MO (Mark Twain stuff), Branson, MO (honky tonk heaven - music shows and tourist attractions - a really neat place). Branson is just south a few miles from Springfield, MO, off I-44 (or old Rt. 66).

BIKE SHOP - POST OFFICE

by Gerry Evans

Banta Modelworks (BMW) released the Light Industrial Building kit (#4072) in 1997 and, due to demand, reissued the kit in 2000. Roger Malinowski's Stoney Creek Designs originally produced the kit in 1:48. Afterward, Malinowski retooled the design into 1:64, and BMW sold the reduced version under its banner. This relationship has spawned a number of BMW/Malinowski kits in 1:64. This article is more about construction techniques than the kit itself since finding one might prove almost impossible. BMW no longer lists this kit on its web site. The footprint is 4.5" x 7.5" and the original price was \$68.95.

BMW/Malinowski kits are known for their hydrocal castings with exacting brick and 3-D stonework. This is the fourth such kit I've built, and I found it to be, with the exception of painting, the least complicated. BMW still shows a handful of other Malinowski-derived kits on its web site. While I normally spray any/all models with rattle-cans, this building is totally hand-painted due to the irregularly contiguous edges between the brick, concrete, and stonework.

WALL BASICS

With steel squares I righted all the walls and attached them to each other using five-minute epoxy. (White glue works equally well but takes longer to dry and requires interior corner bracing.) Once the walls were all raised, I sprayed the entire building with a few coats of Krylon matte-finish clear acrylic. This step is necessary for some of what follows.

BRICK & CONCRETE

Ace Hardware red oxide spray primer comes close to a nice brick color. Knowing I would meet occasions when spraying was impractical, I had the "helpful" Ace paint tech mix up a can of acrylic paint that matched Ace's spray primer. With this, I brush-painted the brick. I kept dipping the brush into some water to keep the paint as thin as possible while still nicely covering the entire exterior post office surface except the concrete along the building top and the cornices above the windows and doors.

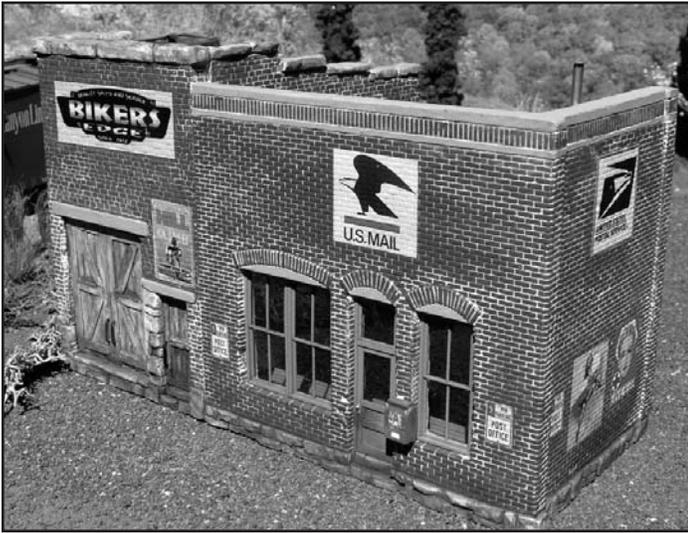
I then painted all the concrete surfaces using Badger Model Flex acrylic concrete. Getting a tiny bit sloppy here was not a problem as a lot of sins were covered up by the next step.

Once the paint was dry, to form the mortar, I brushed on small amounts of pre-mixed drywall compound working one wall at-a-time. I removed most of the "mud" using a combination of a business card squeegee, damp rag, and moistened finger. Once the "mud" had dried, I removed more surface residue with the damp rag leaving as much of the mortar behind as possible. A little spit and a dry rag removed even more residue.

STONWORK

I stained, not painted, the stonework. Initially, I completely covered the stonework with Minwax Honeydew Wood Stain. Once dry, I went over the same areas partially covering them with Minwax River Stone Wood Stain. When the second color was dry, I began to accent the mortar joints between the stones. For is I used a solution of 70% isopropyl alcohol, India ink, and detergent. The proportions for the solution were a pure guess: one pint alcohol, one ounce India ink, one-quarter ounce liquid detergent. This is probably a lifetime supply. I dipped a tiny brush into the solution and lightly held the tip to the mortar joints. Capillary-action drew the solution into the recesses and spread it along all the joint lines. These methods, the stain and the solution, are why I initially sealed the building with Krylon spray. Unless the hydrocal is sealed, the methods won't work well.





CAST-IN DOORS & DOCK SUPPORT

I painted the cast-in doors with raw sienna and the dock support with raw umber, both acrylics. I also applied the ink solution to the cast-in doors to accentuate the wood grain and spaces between the boards.

DECALS

The decals are all homemade. I downloaded the images from the net using Google to find them. I saved the images to two dedicated HD files: Bicycles and Post Office. Then, using an MS Word document, I imported and manipulated the images to fit into the areas where I wanted to place them. I printed the finished document onto Micro-Mark opaque decal paper with an HP color ink-jet. Finally, I sealed the image surface with three light coats of Krylon matte-finish clear acrylic spray.

Before applying the decals, I brushed the brick walls with a light coat of Future Floor Wax. After I partially freed the decals in water from their paper backing, I slid them into place and applied more Future to the decals' surfaces. Once almost dry, with a little help from some thumb-pressure, the Future sank and snuggled the decals into the brick design. I used no decal solvent. Hopefully, if Editor Madden included the right picture, you can see how marvelously this worked.

After the decals were totally dry, I sealed the entire building with Testors' Dull Cote.

WINDOWS & DOORS

I sprayed the windows and doors with American Accents moss green. For the year-2000 version of this kit, BMW included laser-cut glazing and peel-and-stick doors and windows. If the doors and windows hadn't been peel and stick, I'd have used Future, applied with a small brush, as a glazing adhesive. I learned this trick from some gentlemen who model military aircraft and dip their cockpit canopy covers in Future before also gluing them down with Future. The Future removes any scratches, seals the surface, and dries totally clear. It's



a great light-duty adhesive, and, since it dries clear, this is especially true for glazing.

MISCELLANEOUS

The board-by-board flooring in the Post Office came from Micro-Mark and, while a bit pricey, it is exponentially better-looking than scribed basswood. I sprayed the flooring and the rear dock with Minwax Golden Oak Stain. I trimmed the rolled roofing into four-foot widths, cut these into approximate lengths, sprayed the down-sides with MMM 77 adhesive, applied the pieces to the cardboard sub-roofs, and trimmed the edges to fit with a single-edged razor blade. Using Turner Model Works' (TMW) corrugated aluminum roofing and following TMW's ferric chloride (Etchant at Radio Shack) ageing method, I attached the 1:64 4' x 8' steel roofing sheets to the dock roof. As with the rolled roofing, I sprayed the down-sides with MMM 77 adhesive. TMW includes the ageing method with each package of corrugated roofing. (TMW #300, 2/\$4.19, 5 7/8" x 6") I only use this ageing method in the garage with all the doors and windows open. The chemical reaction creates a noxious gas that I'm sure the State of California, in its omnipotent judgment, would determine to be lethal and/or cause birth defects. Lastly, I weathered the rolled roofing, foundation, and area under/around the bike shop smoke jack with finely-powdered chalk applied with a paintbrush. I "powderized" the chalk sticks by lightly rubbing them on sandpaper.



S TRACKS

By Sam Powell

Build a Grade Crossing

The model railroad world is an artistic creation, which, like all artistic creations, draws the participant into an ARTificial world.. In an instant, the participant in an artistic creation is transported into a world of a different time and place, with dramas outside of their ordinary, everyday experience. . No longer are you in the mundane world of the 21st century with daily commute, work and lawn mowing. You are whisked to a different time and place, where a different lifestyle, pace and value system rule.

In the model railroad world you witness the never-ending challenge of man against gravity and friction as tonnage is moved across mountain and plain. At its best the details the modeler chooses to depict can spark the imagination of the viewer so they mentally complete the scene with the many details that the modeler is forced to leave out of the scene for practical considerations. . Our job as modelers is to visually create enough of the essentials to trigger the imagination. From there the imagination takes over.

As the model railroad world gets more sophisticated, and as we get older, and our imagination gets more jaded and less active, we, as model railroad artists must work harder to get the viewer to suspend disbelief and join the scene. Our layouts must be executed with more care, and better detail than the notable pioneer layouts of old. How many of us have watched early TV shows and were amazed at the poor set and scenery design? And yet the early viewers were not bothered by this lack of quality.

If we look back into model railroad history, we get an appreciation for the evolution of model railroad practice and thinking. Frank Ellison, who wrote many pioneering



articles about model railroading focused on the movement of the trains, which came to be known as operation. He used the idea of theater to suggest the larger world beyond the basement or attic. The imagination's image of the world "off stage" was invoked through the purposeful movement of the model trains onto, through, and again off the layout. This required some knowledge of prototype train movement on the part of the viewer. But at its heart was the intend of getting the mind of the viewer involved in the layout world.

John Allen, who was a true pioneer in the development of the visual aspect of model railroading, took the approach that he wanted to eliminate all non miniature elements from the field of vision . He wanted to make it impossible for the viewer to NOT be drawn into the model world. Nothing of the non miniature world was allowed to intrude into the vision of the viewer, even peripherally. This involved extending the scenery clear to the floor and ceiling. As a result, he was limited to modeling real world terrain that was very spectacular, and to some, maybe a bit too grandiose to be believed. So to some, in spite of the modeling mastery he exhibited, to some viewers at least, his approach eventually backfired. Instead of heightening the realism, the grandiose scenery

heightened the skepticism in the viewer's mind. Never the less, the viewer could not help but be drawn into the miniature world he created.

George Selios' Franklin and South Manchester pays modern day homage to John Allen. The extreme amount of detail packed into his scenes works on the principal that the more detailed the scene is, the more readily the viewer is pulled in. The details work to suspend the blocks to the imagination. The highly detailed scenes on his layout do indeed transport one back to the depression era of steam railroading in New England.

Beyond the details in any model railroader, there are certain design features which will pull you into the scene more than others. As I have said before in a previous column, having the trains leave our view and then return captures the imagination and attention as it leads us from scene to scene. The tunnels, mountains and buildings that do this are more than just real world details to add to the model railroad. They can be very carefully placed to heighten the movement and pauses of the eye and to increase personal involvement of the viewer.

One such detail that is often overlooked, yet is easy to create is the grade crossing. Not only is the place where a highway crosses the

railroad tracks at the same level as the tracks (as opposed to a bridge or tunnel) a cool lineside detail, but it is a powerful force drawing the viewer into the model world. It is the most natural thing in the world for people to mentally follow a highway down its path. Most of us drive, or ride on a highway of some kind every day. The edge of the layout is the hardest place for the viewer to make the leap into the model world. There is the hard edge of the layout itself, with its tabletop or benchwork edge that divides the real world from the model world. In some way we must get our minds to accept that we, as full scale, real world humans can get across that boundary of layout edge into model world. Enter the grade crossing. It can pull us right in, across the tracks at the edge of the layout, and into the model world that we have created. A road says travel down me. And a grade crossing is just like the rabbit hole that Alice went down in Alice in Wonderland. If you don't have a grade crossing on your layout, I would suggest you consider engineering one into its design.

To maximize the artistic success, you must try to make the grade crossing logical in some way. The roadway must be incorporated into the scene so it is convincing. Sometimes grade crossings are at the main street of an old town that was laid out in a grid around the railroad's main line, and thus end up right smack across the tracks at a right angle. This means designing enough space for a town of some sort on the inside of the tracks. These downtown grade crossings have been closed in most towns in modern times, but you can model a closed grade crossing just as effectively, and it can serve a similar artistic function. Sometimes grade crossings are out in the country, and the roads can disappear from view around a bend or into a tunnel almost immediately after crossing the tracks. This still serves to pull the viewer across the tracks and into the miniature world.



The grade crossings themselves are constructed in many different ways on the prototype. The materials can be steel, wood, or black top. I have seen photos of brick in grade crossings, but I have to believe that these were slow speed applications where the track was not beat on too badly by the trains. They are seldom concrete. Of key importance is that the material in the grade crossing must be free to rise and fall with the flexing of the track structure as the trains go over it. This means it must float with the track, and be a material that can withstand flexing to some extent. The grade crossing in downtown Gaithersburg, where the old main street crosses the Old B&O (now Chessie) metropolitan mainline has been many different materials over the years, but a diamond tread steel plate material seems to have been the most durable. When it is new, it is very smooth to drive across, and as it gets beat up by both rail and auto traffic it gets bumpier until it gets rebuilt again.

I have used many different materials for modeling grade crossings. Coloring the grade crossing correctly will give the proper impression for grade crossing material. I have used card stock, plastic, plaster, and foam. Plaster was the worst choice, as it caused corrosion on the rails and thus messed with the operations of the trains. One quarter inch foam insulation is a great choice, as it is the right height to rest flush with the rail tops, and it looks the most like black top mate-

rial when it is painted black or dark gray. The procedure for modeling is simply to carefully cut and fit the pieces of material you are using so that it comes almost but not quite, to the top of the rails, and has enough gap beside the rails for the flanges to flow through. If you design your grade crossings well, they can serve as an ad hoc re-railer that will pull the wheels back onto the tracks in the event of a derailment.

There are many lineside details that go along with the grade crossing which are interesting as well. There is usually some protection provided for the motorists. These run the gamut from simple crossbucks to electronically controlled signals, to live attendants to stop traffic with a sign held up by hand. In this case, there will likely be a small shed to shelter the attendant. When I was in Hungary 7 years ago, there was a grade crossing there controlled by a live, human crossing guard. Don't believe for a minute this is a sign of poor trains in Hungary. The trains there run on time, run smoothly, and are fast. No matter what type of crossing protection you model, this detail will stop the viewer's eyes momentarily and add to the artistic rhythm of the layout.

Give this idea a try. The grade crossing is cool, and it works to heighten the artistic impact of the model railroad. And, it gives you a place to display the many fine 1/64 scale autos we have available to us these days.

-Sam

AN HO WALTHER'S PIER WAREHOUSE WILL DO JUST FINE

Photos and story by Roy Hoffman

Scalers have been using appropriate HO scale structures on their layouts for a long time now. The abundance of structures currently available for S has made this time honored exercise less necessary. However, there are always open spots on our layouts that call for certain structures and having more than one way to acquire them is a nice thing. Since S is 136% the size of HO, it is only slightly larger. By following certain criteria you can spot those HO kits that would be the most likely candidates for your S layout.

There are two conditions that I look at when "S"izing up an HO structure:

First, figure the distance in height between floors. I came up with a standard based on New York City's tallest buildings. Ten of these buildings were considered. They aver-

aged 13.5 feet between stories. This scales out in S to about 2 ½ inches. This is the standard I use when I scratch build structures in S for my layout. The HO structures that I've used seem to look good when the distance is two or more inches between stories. It scales out to about 10.5 feet. Anything less than that begins to look too small.

Secondly, another factor to consider is the actual window size. The larger the window, the more you can get away with by going under our 2 ½ inches between floors standard. Considering the above factors, many of the City Classics and Walthers Cornerstone structures would fill the bill.

I was recently visiting a local hobby shop, Mainline Hobbies in Blue Ridge Summit, PA near Gettysburg and Ft. Richie, MD. In their consignment section was a Walthers kit

for their series of waterfront structures. It was the Municipal Pier Terminal. They offered 2/3 off on the price, so I took a chance. I was very pleased with the result. The PWRR now has a nice pier terminal for the Delaware River waterfront. There's actually 2 ¾ inches between the two stories making it dead on for S.

As usual, the doorways are too short for S people. I also had to make another modification because I wanted to run box cars into the building. I had to take about 3/8ths of an inch from the bottom of the gray window over the doorway to accommodate S box cars.

Now I have a nice waterfront structure for the Penn Western. By following these few simple factors, I'm sure that you'll find a whole range of suitable structures from the world of HO.





Roy Hoffman utilized the Walther's HO Municipal Pier Terminal building kit for his S Scale waterfront scene on his Penn Western layout. The overhead gantry crane was kitbashed. The girders are from a discarded toy and the top is scratchbuilt.
Photos by Roy Hoffman



Meet an S Gauger

George Reneris
By George Reneris

The Snoqualmie, Mohonk & Panguitch RR

The SM&P is S gauge. It has a control panel that, with a flip of a few toggles, will allow 3 separate transformers to run either AC or DC, or in any combination that the operator wishes. Three trains can be run individually on these 3 loops, and all loops are interconnected via turnouts. The switchyard is controlled by a fourth power supply. another switch allows all tracks to be run by one transformer, and yet another toggle converts the whole layout (about 350' of track) to a home-made wireless setup. Three toggle switches bypass the AC and DC modes and converts everything to MRC Wireless DCC.

Most steamers have smoke and chuff. In AC or DC mode the en-

gines that have full sounds are a 4-6-2 Royal blue and a home-built camelback. All five DCC locos have sound. They are a Galloping



Goose, an SW1, an F3, and two converted Flyer steamers - 4-4e-2 and 4-6-2.

The main line is 150' in length. It travels up 3 levels through a moun-

tain pass and then descends to the "subway" located beneath the table. The line that passes through the "subway" can be routed to the main level in three different locations.

The layout table is 42" high and is 11' x 21'. It's an M shaped layout with a slant. The open space on the left M is connected with a lift bridge. The left end of the M is also connected to shelf sections which

run about 37' along the walls. The shelves contain a yard, turntable and roundhouse.

Besides many action accessories-George has lots of animation scenes





on his layout. These include moving hot air balloons, exploding dynamite, lumber jacks, children on playground equipment, a Goodyear blimp with moving messages and even an outhouse with a talking occupant. Most accessories have fascia mounted push buttons and controls so spectators can operate them. Handouts titled “can you find” add enjoyment and extra ap-

preciation for the SM&P. Good tips for show layouts too -ED.

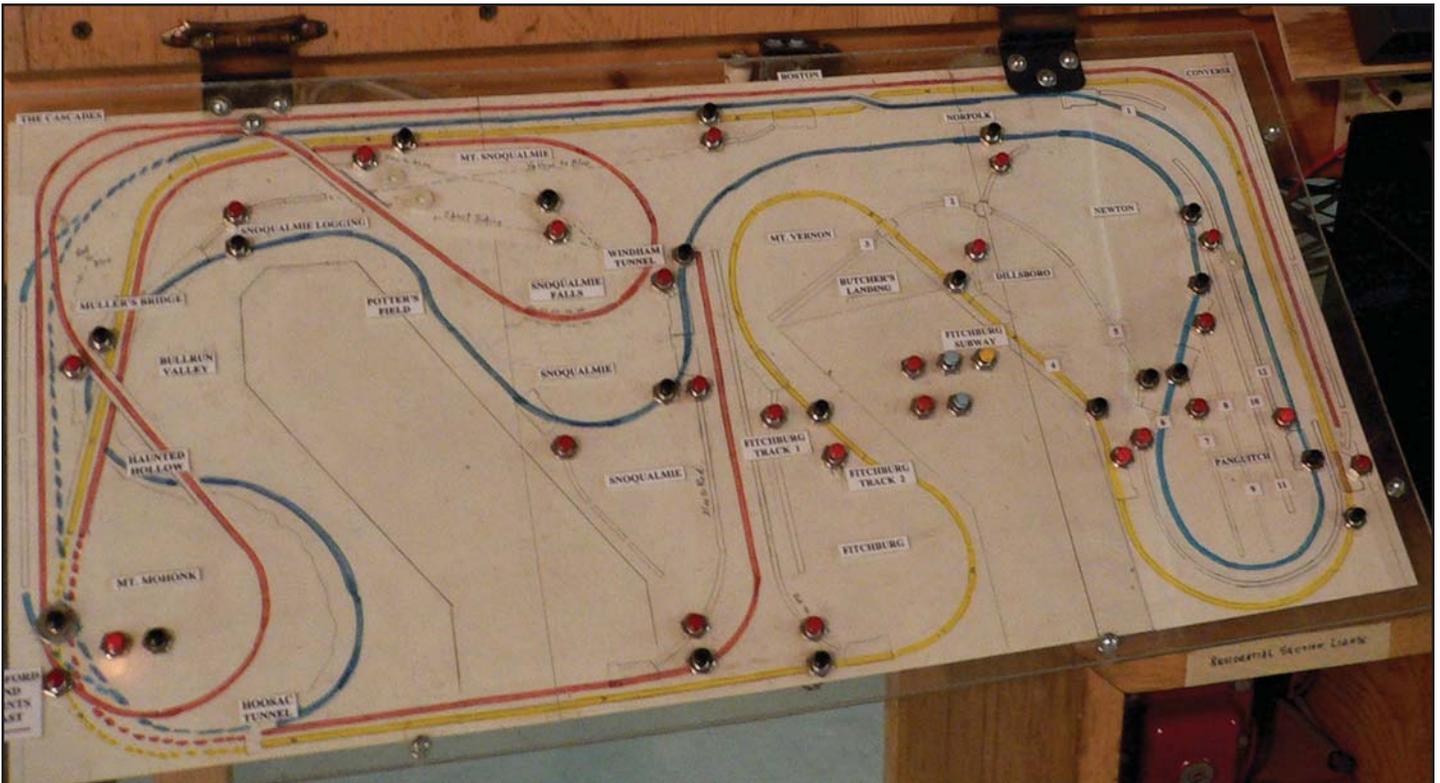
About 10-18 operators can be kept busy running trains or operating accessories.

To sum it up: this is not a rivet counter layout. It is a layout that blends a high amount of detail,

scenery, lighting with lots of action, yard switching and industrial switching.

George won awards for several of his accessories at the Lowell NASG convention. Two entries were on page 17 of the Oct. 2008 *Dispatch*. The *S Gaugian* featured his 4-4-2 on page 17 of the Nov./Dec. 2008 issue.





Above is the layout diagram on the control panel of George Reneris's SM&P AF hi-rail layout in Massachusetts. Below - 2 levels and a drive-in theater highlight this scene. George has lots of animation. *Photos by George*



S TRACK PLANNING PART VI By Jeff Madden

The Manhattan Beach Railway in S

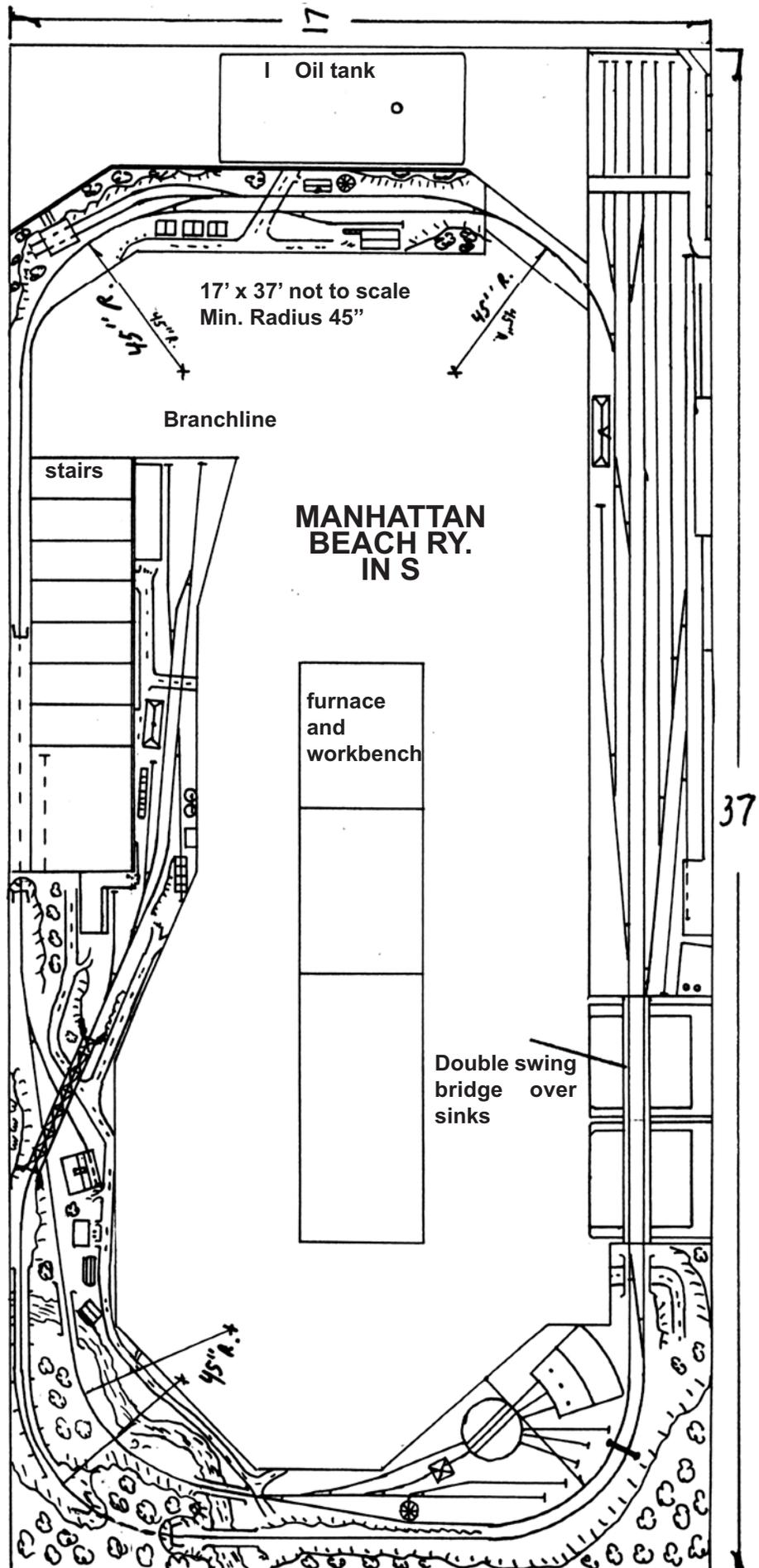
As promised here's another S layout based on an old O scale plan. In the classic 1940s series in *Model Railroader*, 50,000 Spikes, George Allen and Ernie Huebner told about the O scale outside third rail Manhattan Beach Ry built in Ernie's basement. Both were O scale pioneers and were members of the New York Society of Model Engineers.

The layout shown at the right is similar to their plan shown in the December 1941 issue of *MR*. Again, I left in the usual bungalow basement obstacles. The size in S is kept at about the same size as the original O scale layout, but by tweaking the radius, the width could easily be squeezed down to about 11 or 12 feet and the length could be shortened some as well.

The original layout didn't have a branch line. Instead it had the typical high line and low line. I added the branch line for operational interest. The original Manhattan Beach used the high line as the main and the low line was stub ended. I also added a passing siding at the top end of the layout with sidings for a mine and an industry or freight house.

As Ernie and George admitted, their O scale layout was basically a test track with scenery. Like the Chester Valley layout last issue, the Manhattan Beach is very doable in S. It's a basic rectangle with continuous running, a branch, a sizeable yard and an engine terminal. Operation would be simple - run a mixed train up and down the branch to and from the yard, and then run some mainline trains around the level loop.

If you have a less cluttered area to work in, so much the better, but this shows what you can do when forced to overcome obstacles.



The **S**traight & Narrow

By David L. Heine

News and Reviews on the Narrow Gauge Front



Back to Basics - Scale vs. Gauge

I was looking through some back issues of the *Dispatch*, and realized my first column appeared in the February, 1996 issue. Since it's been awhile, I decided to revisit the basics of modeling narrow gauge railroads in S scale. This article is about scale vs. gauge and the model railroading advantages of narrow gauge railroads.

First, one must understand the dif-

ference between scale and gauge. Scale is the ratio of the models to the prototype, in our case 1:64 or $3/16" = 1'$. I realize that "scale" also is used within the S community to mean more accurate, non-hirail/non-AF models. However, in reality, AF is also S scale. Gauge is simple the distance between the rails.

The common modeling scales have been given letters, including our

beloved S scale, and when stated by them self, imply standard gauge (4'8-1/2") models. Since there were multiple gauges used, a shorthand nomenclature has been developed. Using Sn3 as an example:
S – Scale, either letter (HO, S, etc.) or fractional scale (1/2", 3/8", etc.)
n – For narrow gauge (w for wide gauge)

3 – Gauge in feet if single number (3, 2); in inches if double digit (30, 42). I know this is inconsistent, but this is the common nomenclature.

Sometimes modelers use one scale's track gauge as narrow gauge in a larger scale. For example, I have seen HO gauge used for Sn42, On30, 3/8"n20 and Gn15. S gauge can be used for On42, 7/16"n2, Gn20 and Fn18.

For those modeling S narrow gauge, the following common narrow gauges can be used:

Sn42 – Using HO gauge

Sn3 – Using its own gauge

Sn30 – Using TT gauge (also HOm)

Sn2 – Most commonly using HOn3 gauge, which is 2+inches too wide. N gauge is also sometimes used, but it is too narrow, so it is better suited for industrial tramways.

Although many different gauges were used for narrow gauge railroads, the majority of narrow gauge railroads within the United States were 3' gauge. So it is reasonable



to expect that Sn3 would be the most common S narrow gauge, which is the case. 42" gauge railroads were common within the former British colonies, so Sn42 is used by many modelers of those railroads. Sn42 was also used in the early days of S narrow gauge by some pioneer modelers to model 3' gauge railroads since they could use HO mechanisms and trucks without regauging. Also, there was significant 42" gauge track in Newfoundland, which lasted into the diesel era. There is a small, but enthusiastic, group modeling in Sn2, mostly following the 2' gauge railroads in Maine but also the Gilpin Tram in Colorado.

It should be obvious that there is no such thing as an Sn3 building, only S scale buildings. Don't laugh; I have heard someone asking where they could get On30 buildings! Other than locomotives, rolling stock, and track gauge, everything else is the same.

The narrow gauge railroad building boom in this country was between 1870 and 1883, during which 12,000 miles of narrow gauge track was laid, most of it to 3' gauge. They were built mostly on the belief that they were cheaper to build and operate. They were cheaper to build, mostly because they built as substandard railroads, with less grading, resulting in narrower roadbeds, steeper grades, and sharper curves. They also used lighter rail and bridges for the smaller and lighter equipment. All total, about 350 common carrier narrow gauge railroads were built in this country. Note that this num-

ber does not include the many narrow gauge railroads that were built for industrial purposes, such as logging or mining railroads. Some of these routes are still being used as parts of modern standard gauge railroads.

Note that narrow gauge railroads have attributes over standard gauge railroads that make fitting a model railroad into the available space easier. These include:

- Sharper curves
- Steeper grades – 4% was a common maximum for mountain railroads, instead of the usual 1-2% on standard gauge railroads. Examples of steeper grades can be found.
- Shorter cars – Freight cars are generally 30' or shorter. Passenger cars are generally shorter than 48'.
- Shorter trains – Generally trains were short requiring shorter passing sidings, etc.
- Smaller motive power – Motive power was generally smaller.

Combine all this and you can have a 16-car train being pulled by two 2-8-0's being more realistic than a 30-car train pulled by an SD60. There are actually two modeling advantages, shorter trains and a higher motive power to rolling

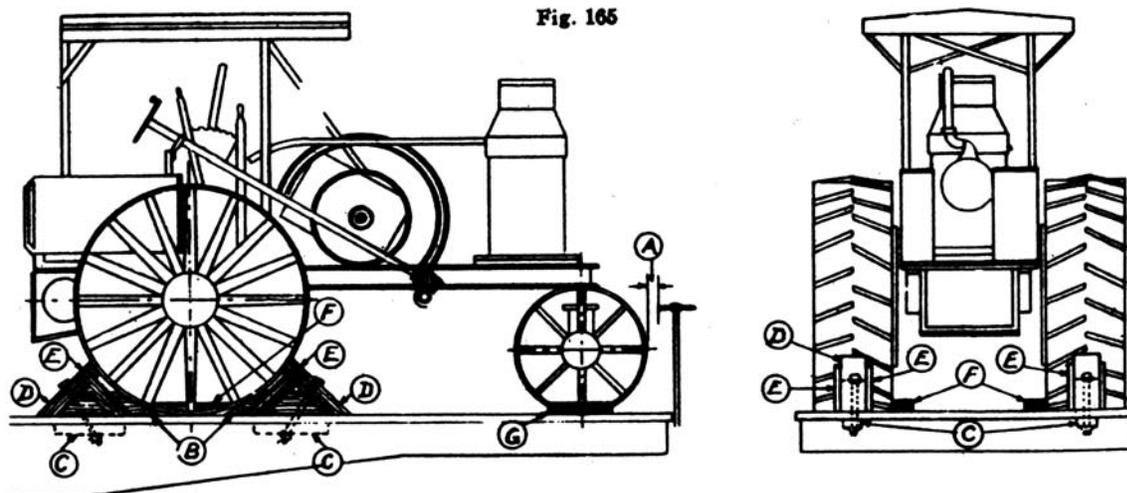


stock ratio that many model railroaders prefer.

Like many others, I think S is the best scale for indoor narrow gauge modeling. It is large enough to build detailed models with moderate modeling skills and to operate reliably. It is also small enough to build a realistic layout in a reasonable space. In Sn3, you can fit roughly the same amount of railroad in a space that you can with HO, it is just that the buildings are larger and the details are more visible.



Fig. 165

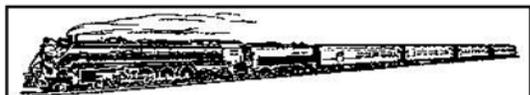


ENGINES — FLAT CARS

Item	No. of Pcs.	Description
A		Brake wheel clearance. See Fig. 2.
B	2 ea. rear wheel	6 in. x 6 in., one bolt, dia. $\frac{3}{4}$ in. and six 40-D nails.
C	2 ea. rear wheel	4 in. x 4 in. x 18 in., hardwood cleats, or $\frac{1}{2}$ in. x 4 in. x 12 in. plate.
D	2 ea. rear wheel	4 in. x 6 in. x 36 in., nailed to floor.
E	4 ea. rear wheel	1 in. x 6 in., nailed with 10-D nails to Items "B" and "D."
F	1 ea. rear wheel	3 in. x 4 in. x 18 in., nailed inside of and against wheels with five 60-D nails
G	1 ea. front wheel	3 in. x 4 in. x 18 in., outside of and against front wheel, nailed with five 60-D nails.

Brake and clutch must be tightly set.

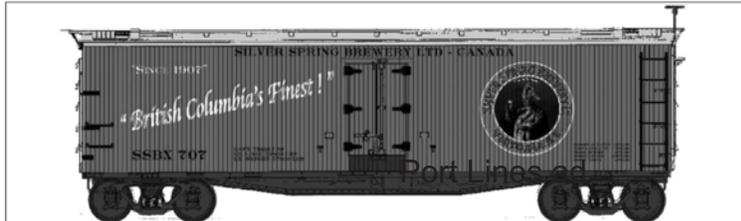
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THE CLUB SANDWICH

By Dave Pool

The **Bristol S Gaugers (BSG)** were scheduled to meet at the home of Chet Brown for their January meeting. Tom Robichaud is President of the BSG; Helen Lenart, V.P.; Chet Brown, Treasurer; and Dick Connors, Secretary of the club. Member Alan Coughlin ably edits the club newsletter. The club set up a layout at the New England Model Train Expo, held at the Best Western, Royal Plaza Trade Center, Marlborough, MA. on Dec. 6/7. The event is advertised as “The Holiday Season’s Largest Train Show”, and will include: operating layouts, over 220 tables for dealers, Santa, a Boy Scout merit badge clinic, free parking, and available food service.

The club held their annual holiday party on Dec. 14, as hosted by Michael, Deane, and Matthew Greene. The **BSG** also operated a layout at the Railroad Hobby Show sponsored by the Amherst Railway Society on Jan. 24/25, 2009 at the Eastern States Exposition, West Springfield, MA. This show was held in four buildings at the fairgrounds, and it always draws thousands of visitors. The club was considering the set up and operation of their modular layout at the First Annual Westside Model Train Show to be held on Saturday Feb. 28, at the West Springfield Middle School, West Springfield, MA.

The **Connecticut S Gaugers (CSG)** set up and operated their modular layout at the Cheshire High School Ram Band Train show on November 23, 2008. The set up was coordinated by Steve Kutash, V.P. of the club in charge of the module displays with the help of other members who brought modules to the convention. The club layout normally includes tracks operated with AC, DC and DCC power supplies. Operation with DCC was recently instituted on their modular layout.

The **CSG** met at the home of Bob Davis for their Dec. meeting in Old Lyme, CT. on Dec. 6, 2008. The host has a large detailed and scened layout with the ability to operate long trains. Bill Fuhrman is

club President, Steve Kutash, V.P.; Craig O’Connell Sect’y/Editor; and your column editor is Treasurer of the club. The June issue of the club newsletter included a listing of 52 paid members for the year 2008. Several additional members have joined and renewed their membership status since the listing was published. The club is purchasing club hats and shirts (both w/ & w/o pockets) that have the name of the club on them. Steve Kutash and Bob Comstock are coordinating the ordering of the shirts. The club was planning on a set up of a layout at the Cheshire High School Train Show scheduled for March 1, 2009 at the high school. Steve Kutash will coordinate the club’s attendance and layout set up.

The **Pioneer Valley S Gaugers (PVSG)** met at the home of Larry Covey, on November 22. The host has an extensive collection of model trains in many gauges, and Larry had an S gauge train operating for the meeting. Club member Steve Allen demonstrated some Ribband brand material that he uses for curtains in model buildings. The material was purchased at a local Michael’s Art & Craft store. Frank Grano edits and distributes the club newsletter, and Dave Plourde, founder of the club, is a contributing editor. Frank hosted the club members for their December meeting on Dec. 20, 2008. Recently Dave will be taking over the newsletter responsibility, as Frank has other commitments. Several club members met at the Steaming Tender Restaurant with several members of the BSG to have lunch. After lunch those attending visited several layouts to share their Club members S modeling ideas. Steve Allen, Charlie Bettinger, and George Reneris from the **PVSG** and Tom Robichaud, Bill Clark, and Helen Lenart from the BSG attended the event.

The *Canadian S Scale Quarterly (CSSQ)* is the official publication of the **Canadian S Modelers**. The most recent issue had photos of a 2-10-4 steam loco with the caption “the above model is S scale, but we don’t have any information

on it”. The photo of the model has what appears to be the prototype on a turntable in another photo on the same page. Helen Hicks edited the most recent issue of the *Quarterly*. All submissions to the newsletter should be sent to Alex Binkley, in Ottawa, ON, Canada. S Scale Workshop member Pete Moffett is recovering at home from a freak accident that occurred in mid-October, 2008. Pete was on a motorcycle tour with three other companions, in Vermont (U.S.A.) when a passing log truck trailer tipped over and spewed it’s load of logs at the group. Pete was the most severely injured of the people on the tour. He is an NMRA Master Modeler and had plenty of visitors inquiring about his condition at a recent train show in Toronto, where Canadian S members were in attendance. The recent issue of the newsletter included an article by Barry Silverthorn on the Middleton & Sons Warehouse and Transfer model kit by Grand River Models.. Denis Fortier authored an article on RR speeders, and his operation of one on Train Simulator software on his computer that was included in the recent *Quarterly*. Helen Hicks did a wonderful job with the most recent issue of the *Quarterly* (No. 64, Jan., 2009).

The **Western N.Y. S scale Association (WNYSSA)** has been having work sessions on Wednesday evenings at the home of Paul Wachowicz getting their 4 ft. x 8 ft. layout ready for displays at shows and other events. The club set up and operated their layout at the GSME Show, on Nov. 9, 2008 at Batavia Downs, Batavia, N.Y., and the TTOS Show at the Leonard Post, Cheektowaga, N.Y. on January 24/25, 2009. This was the first display of the layout after extensive work by the membership. The club renewed their NASG booster ad, which appeared in the recently published directory issue. The club members met at the Pegasus Restaurant, Hamburg, N.Y. on Nov. 23. There was a TTOS train show on the same date which members could attend after the meeting. Paul Wachowicz is scheduled to host a work session on Dec. 3 for the club’s small layout. The new layout was set up and displayed at the TTOS Train Show held on Jan. 24/25 at the Leonard Post in Cheektowaga, N.Y. The club planned to meet at the Danny’s South restaurant in Orchard Park, N.Y. for their February meeting. The **WNYSSA** is planning to set up and operate a layout at the

GSME Train Show on March 29, 2009 at the Batavia Downs, Batavia, N.Y. with the Rochester Area S Gaugers.

The Rochester Area S Gaugers Club (RASG) has a web site managed by member Bill Johnson, and it is used to display photos of recent events that the club has planned and attended. The club met at the home of George Cole on Sunday, Oct. 12 for their monthly meeting. The website includes a scrapbook and schedule of events page.

The Waybill is the official newsletter publication of the **South Jersey S Gaugers (SJSG)** and the club meets on a regular basis on the first Friday of the month, at the Stratford, N.J. Senior Center. Play Trains events held by the club are meets at a member's home to operate a layout or visit to a model RR event or place, without any club business meeting taking place. Officers of the club are: Hank Worrell, Pres.; Mike McConnell, V.P.; Steve Politowski, Sec'ty.; Joe Balcer, Treasurer; and Jim Oliver, Ass't. Treasurer. Mike McConnell edits the club newsletter.

The club is to be commended on their use of standing committees that are responsible for various activities and duties of the club operations. They have a Layout Committee, History Committee, Clinic Committee, Trip Committee, all that perform various duties of the club activities. This organizational set up allows (or makes operational) various members to participate in the events of the club. One person is not responsible for planning, organizing and over-seeing the club activities.

The **SJSG** newsletter is edited by Michael McConnell. The club meetings typically have clinics related to the construction and maintenance of S scale trains, or other topics of interest to the members. Don McGinnis gave a clinic on weathering rolling stock at a recent club meeting. The May meeting clinic was on scenery or weathering, for the members interest. Club member Tom Williams gave a clinic at the July club meeting on control panel orientation, and club president Hank Worrell gave one at the October meeting on the repair of AF smoke units. An operating AF loco was demonstrated to the delight of the members attending the meeting. The club displayed their layout at the Magnolia, N.J. Historical Society benefit event on Nov.

22. The June/July issue of the Waybill, club newsletter included an article by John Aaron on scenery and the use of stationary items on the layout. John heavily illustrated his article with photos to depict examples of his scenery design ideas. John suggested and volunteered to formulate a "press Kit" that could be distributed to persons seeking information about the club at train shows or other events. The Dec. meeting of the club was held on Dec. 12, as the Senior Hall where the club meets has special use for the Christmas holiday. The **SJSG** supported the request of the **NASG** to set up and operate their layout at the World's Greatest Hobby Show, held on Jan. 3/4, 2009 at the Greater Phila. Expo Center, Oaks, PA. The club also set up and displayed their layout at the train show held on Dec. 20/21, 2008 in Tuckahoe, N.J. at the Upper Twp. Community Center. John Aaron wrote a review of a book authored by David Baldacci entitled "The Christmas Train" that was included in the recent issue of the *Waybill*. John Bigley wrote an interesting article on passenger stations in a big city that appeared in the recent issue of the newsletter.

The Neshaminy Valley AF Club (NVAFC) held their December meeting at the home of Mike Ramsey. Club members welcomed Tom Keegan after his absence and he has overcome some medical issues. Mike Ramsey coordinates the club activities and distributes the club newsletter via e-mail. Bob Hegedus was recently voted upon as a new member of the club, and attended the meeting at Mike Ramsey's. Bob is a AF collector and operator who lives in NE Philadelphia area. Susan and Jim Duffy held the January, 2009 meeting at their home on January 9th. Club members made their annual trip to the Lancaster, PA. area on Dec. 27, 2008. Members and Spouses went on the trip, that included the PRR Museum, and other RR related attractions in the area, including the Choo-Choo Barn, with a meal at the Shady Maple and Bird-in Hand restaurants.

The Susquehanna S Gaugers (SSG) were hosted by Mary Jane and Dick Purnell for their meeting on November 9, 2008. The club was planning on setting up and operating a display layout at the annual "Toy Train Expo" in Williamsport, PA on Nov. 22/23, 2008. Mark Anderman coordinated that event, with help from other

members of the club. The club members were considering a visit to the private layout of Bob Weaver, of Weaver's Model Trains company. Dick Purnell was to coordinate the visit scheduled for January 18, 2009. Jim Ingram demonstrated the use of magnetic block controls to operate multiple trains and engines on the same loop of track. Dave VanGilder issues publicity info on the club and their activities. Dave conducted a business meeting including the club members at the Nov. meeting.

The Pittsburgh S Gaugers (PSG) held a *Coffee & Trains* get together at Kings Restaurant, in Harmerville, PA. on Jan. 12, 2009. These meetings are well attended and are separate from the regular meetings of the club, held in member's homes. The Dec. *Coffee & Trains* get together will be held on Dec. 15. There was no regular meeting scheduled for the month of December. Jonathon Knox coordinates the club activities and distributes the club newsletter via e-mail. The January club meeting was held at the facilities of Kennedy First Alliance church, near McKees Rocks, PA. (the church of Jonathon Knox). At recent club meeting it was discussed whether there should be a formal presentation at the regular meetings, and the members present decided to not have any formal presentation, but such could be used if the show and tell and discussion topics became exhausted. The club has found a good location for meeting locations are church social halls or fire dept. halls. A member of the club can host the meeting and coordinate the availability of the hall.

The club members discussed possible programs for their meetings, and a lot of very good ideas were presented at one of the recent meetings. These included having a white elephant sale where members could sell their unwanted train-related items, and a clinic on assembly of a wood laser kit, where the club would provide, and members participating would purchase a small building to practice on as part of the clinic. The club attended and set up an operating layout at the Greenberg train show held on February 7/8, 2009 at the Laurel Highlands Event Center, near Westmoreland Mall, Greensburg, PA. Jon Knox and Roger Schneider coordinated the club's attendance and operations at the show. The club is making plans to attend and set up a layout at the Mid Mon Valley

RR Club Train Show on Feb. 22, 2009. The club has two operating layouts, including a high-rail layout and a modular layout both of which can be used to display trains at shows and events. The club members are planning to upgrade and maintain both layouts by changing scenery items and upgrading the electronics. The club was invited to bring and display a layout at the NMRA Jamboree to be held at the Robert Morris University, on Saturday, March 21. The club members are planning on possible attendance at the Great Scale Model Train Show and ALL-American Hi-Rail & Collectors Train Show in Timonium, MD. in early April.

The Baltimore Area AF Club (BAAFC) club members met at the home of David Whitworth on Jan. 25, 2009, in Edgewater, MD. The club celebrated its 15th anniversary in 2007, being first formed in 1992. The club set up and operated a layout at the Great Scale Train Show, on Jan. 31 Feb. 1, 2009, in Timonium, MD. A recent club newsletter issue contained an article by David Avedesian on How to Design a Helix for Your Operating Layout. The construction articles are included in the newsletters as a separate page, punched for easy saving in a three ring binder.

The club members have decided to have train repair sessions at their meetings to make repairs on train equipment, and teach the members how to perform certain train repair techniques. The club does civic events with their layout which is commendable, and recently set up a layout at the Johns Hopkins Hospital, in Baltimore, MD. These events are well received by the hospital personnel and patients, and visitors to the hospital. The club set up and operate a layout at the B & O RR Museum from Dec. 4 through Dec. 7, 200. The displays were from 10:00 A.M. until 4:00 P.M. most days.

The Hoosier S Gaugers (HSG) held their Indianapolis S Train Show (their fifth show) last year. The event included a pre-show get-together at the Shapiro's Restaurant, with about 40 guests and friends attending. The show included S vendors and door prizes. There were two operators which was used to operate a "wide-load" train which was enjoyed by those attending the event. Alan Evans of Gahanna, Ohio brought and operated the 2

ft. x 12 ft. NASG switching layout that operated with DCC. This year's show (2009) will be held on Saturday Oct. 10, Columbus Day weekend. Club members displayed a 4 ft. x 6 ft. layout at an NMRA show in Noblesville, IN. on Jan. 25. Charles Malinowski, Mick Hinkle and Jerry Schnur among others attended the event and operated the layout.

The Chicago Flyer is the official newsletter of the **Chicagoland Assoc. of S Gaugers, Inc. (CASG)** that meet on Friday evenings on a monthly basis. Will Holt is president of the club; Joel Lebovitz, V.P.; Joe Taylor, Secretary; Bruce Lorence, treasurer; Joe Craig, Officer-at-large; and Bob Brown, Joel, and Phil Kosin serve as Trustees. The officers were elected at the December, 2007 meeting of the club. New elections are underway for several officers positions and one trustee position. The CASG was founded in 1970, and incorporated in 2008. Past meetings of the club have been held at the St. Paul Lutheran School. The club set up and operated one of their layouts at the Southland Train Show, held on October 25/26, at R.L. Richard High School, Oak Lawn, IL. John Griffin, a former club member brought a scratch built Schnabel heavy duty load RR car. The prototype is used to transport extremely large heavy loads and the prototype John modeled is capable of carrying 1,700,000 pounds. The model was over 5 and a half feet long.

The **CASG** members have been working on their modules, and have wiring done so that they can operate trains using standard AC, DC, DCC, Trainmaster Command Control, and Locomatic control by switching power supplies. Joel Lebovitz has been coordinating improvements on the club modules. The club held a meeting at the St Paul Lutheran Church hall, Melrose Park, IL. on Nov. 17. The club is looking for an alternate location for their meetings. The members participated in the Great Midwest Train Show, to be held on Dec. 14, in Wheaton, IL. by bringing and operating a layout at the show.

The Miami Valley S Gaugers (MVSG) recently held elections resulting in the following officers of the club: John Clifford, Pres.; Dennis White, Treasurer; Tony Garza, Corres. Sect'y.; and Rick Smith, Board Member. Tony Garza faithfully distributes the club minutes of club meet-

ings and events. The club's Christmas dinner was held at the home of Mary Jane and Larry Beam's home on January 10, 2009. The Feb. 7, 2009 meeting of the club is scheduled to have a clinic on tree making by Dave Decker, and will be held at the home of Tom Hartrum. The club is seeking members houses for the forthcoming meetings in 2009. The next show for display of the club layout will be the Miami Valley Rail Festival, Carillon Park, in June. Larry Beam hosted the club for a meeting on Jan. 10, and eleven members attended, including the host. The club is planning module repair sessions during March, April, and May, to be held at the home of Larry Beam. The club plans to set up and operate a layout at the nursing home in Eaton, OH. on May 16/17. Bob Guckian is to investigate the possibility of the club members visiting a large S layout in Oxford, OH, possibly as an event to be done with the club picnic in the summer months. Nominations were received for the upcoming elections to be held at the home of Tom Hartrum on Feb. 7.

The Stateline S Gaugers (SLSG) has a newsletter ably edited by Vera Flood, and the various issues provide interesting facts about railroads and the club activities. The newsletter includes listings of RR and model RR events that might be of interest to the club members. SLSG club officers include: Jim Larson, President; George Sorensen, V.P.; Vera Flood, Secretary; Dave Pippett, Treasurer; and Dave Wilma, Dave Oberholtzer, and John Sevall, Board of Directors members. The club is planning for the 2009 S-Fest which they are scheduled to host. A committee was formed which would have the responsibility of the event and have the club president Jim Larson as an ex-officio member. Doug Allen has agreed to be chairman of the event, and was included in the seven members who will form the committee to plan and organize the event.

The club is looking for a place to set up and work on their modular layout, and they are actively seeking new club members. A mailing was made to all known S gaugers in the area where the club holds events and meetings. No replies were received from any possible members. David Wilma suggested that brochures be placed in the local libraries to solicit possible new members. The annual Christmas party of the club was held on Dec. 20, at the Firefly Restau-

rant, Loves Park, IL. George Sorensen coordinated the club event. The club attended the Midway Village Train Show, on Nov. 1 & 2 in Rockford, IL. Roger and Vera Flood brought a 4 ft. x 8 ft. layout and Phil Howe displayed some AF All Aboard panels. Club members were present to help with the layout set up and operations.

The club brought and displayed their modular layout at the Belvidere Train Show on Dec. 6. They set up on Friday before the show the following day. A news item in the recent issue of the club newsletter explains that the oldest RR depot in the Midwest is in Mazomanie, WI. and was constructed in 1857. It presently houses a library, and was built for the Milwaukee & Mississippi RR.

The Southeastern Michigan S Gaugers (SMSG) has 46 members as reported at a meeting last year. Club member Gordy Michael is the rolling stock coordinator for the club and he keeps track of the equipment that is operated at the various show displays that the club makes. Gaylord Gill and Dave Held participated in the "Interstate All-S Operating Weekend" that was held last April and written up in the *Dispatch* June, 2008 issue. Photos on the event disclose some pretty awesome layouts that would be good for articles in the magazine. The club is planning for the 2010 Spring S Spree, to be organized and sponsored by them on April 30/May 1, 2010. The event will be held in the Dearborn Community & Performing Arts Center., near the Amtrak station in Dearborn, MI. Flyers have been printed and the organization of the event is underway.

The Northern Ohio S Scalers (NOSS) held their holiday party at the home of John & Pat Henning in Valley City, OH. on Jan. 4, 2009. Jack Sudimack coordinates the club's activities, and John Henning edits and distributes the club newsletter issues. John is presently investigating the possibility of distributing the club newsletter via the internet. The club members are planning improvements on and use of their modules. Jack and Sharon Sudimak held the January club meeting at their home in Medina, OH on Jan. 25. John Henning and Pat will host the club for some sessions to work on their modules. Gary Ippolito wrote an article on the history of the **NOSS** that was published in the club newsletter describing how the club was started ten

years ago. The club has 45 ft. of point-to-point fully-sceniced switching modules that they can display at various events and train shows.

The Badgerland S Gaugers (BSG) held the first meeting for the new year on Jan. 25, 2009. The club is planning to have clinics at more of their meetings to increase the attendance. The club set up and operated a layout at the Titledown Train Show, in Green Bay, WI. recently. Ron Schlicht coordinated the club participation by bringing the club's small layout to the show. Members of the club helped with the layout set up and operation. New officers elected recently include: Steve Lunde, President; Richard Wade, V.P.; Jeffrey Young, Treasurer; and Ron Schlicht, Sect'y./Editor of the club newsletter. Directors of the club include: Dick Kloes, Ray Puls, and Trumann Garrett. Ron Schlicht coordinates the club meetings, and ably edits the club newsletters. A recent listing of the membership of the club included 74 members. Jeffrey Young hosted the club meeting in December in Menomonee Falls, WI. Election of officers and Directors was to be held at that meeting. Ron Schlicht coordinates the meeting schedule for the club meetings generally held at the members homes.

The AF S Gaugers of the St. Louis Area (AFSGSLA) held their February 6 meeting at the home of Barry Dolan in Ballwin, MO. Moe Berk coordinates the club events and Gary Mueller ably edits the club newsletters. The **AFSGSLA** set up and operated a layout at the TCA Train Show held at the Lutheran Church Hall, Olivette, MO. on Jan. 31. The club recently established an e-mail address, at: afsgsla@sbcglobal.net. Club member David Stevens has agreed to coordinate the club meeting locations and times. The club planned to display their sectional layout at the Dupo Train Show, Dupo, IL. on Feb. 14. A memorial service is scheduled honoring Laura Riggins, wife of Gary Brandenburger to be held on Feb. 14 at the Florissant Presbyterian Church, Florissant, MO.

The Inland Empire S Gaugers Assoc. of the Pacific Northwest (IES-GAPN) is celebrating their 40th anniversary this year and will have an S gauge tank car made by American Models. Bob Bowen is coordinating the design and ordering of the car. Robert Bowen hosted

the club members for their Nov. meeting in St Maries, ID. on the 8th of the month. Jon Kettner ably edits the club newsletter, and coordinates the club activities. The club had a meeting/banquet on Dec. 13, at Conleys Place restaurant. This will be the second banquet of the year for the club. Another banquet will be held in March, 2009 for those who cannot attend the one this year. In past years, weather has been a factor on the attendance and the banquet was moved to March to insure good attendance at the event. The club met at the home of Jon Kettner, in Fairfield, WA. on January 10, 2009. The members of the club have been working on their layout in preparation to its display at a swap meet on February 15, at the Spokane Community College.

The Flyer Fest West is scheduled for May 1-3, 2009 in the Sacramento, CA. area. The event is being organized by the **Sacramento Valley AF Club (SVAFCl)** and will feature a Friday night reception, model contest, trading hall, and train repair demonstrations and aid to solve train problems of those who bring them to the event.

The Dec. meeting of the **Southern Calif. S Gaugers (SCSG)** was a holiday get together, held at Marie Callender's restaurant. Twenty eight members and friends attended. Jeff Kruger is the coordinator of the club activities. The club is in the process of designing standards for their modules and constructing same. Module sections will be made by those members so inclined, and a special meeting will be scheduled for module construction. The club won its first award, a first place blue ribbon and a box of candy for the best depiction of the Halloween Theme among the eight layouts at the recent Oct. 25/26 train show as sponsored by the Inland Terminal Railroad. The club recently displayed a layout at the Cal-Stewart Train Show, in Pasadena, CA. on Nov. 21/23. The club was considering train shows that will be held in 2009, at which the club might attend and display a layout. Nick Mescher was commissioned to investigate such shows. Jeff Kruger was scheduled to host the club meeting in January, 2009, at his home.

The San Diego S Gaugers (SDSG) is tentatively planning to have an initial set of modules constructed and operating in time to display at the Great Train Expo,

in Del Mar, CA. on December. 13/14. In order to meet this goal, the club monthly meetings will become module building work meets for as long as needed. Members worked on their modules in the interest of having a layout ready for the Dec. event.

The **Bay Area S Scalers (BASS)** held a meeting at the home of Mike Schwab, in Santa Cruz, CA. on Nov. 15. The club set up a display layout at the O Scale West +S Train Show on Jan. 29-31, 2009, held at the Hyatt Regency Hotel, Santa Clara, CA. Graham Henry is coordinating the club attendance and layout set up at the event. Joe Visintine is distributing the 2009 BASS calendars to those members who ordered them. The club will not be setting up a layout at the Dunsmuir RR Days celebration next summer. John Rolston hosted the club meeting in January, 2009 in Sonoma, CA. Lee Johnson edits the club newsletter, called the *Bass Waybill*. Graham Henry publishes and distributes the newsletter issues primarily by e-mail.

Your club column editor wishes to thank those club members and clubs that submit information on the activities and events that you organize and attend. Please submit information and newsletters for the column to David Pool, 11 Bittersweet Trail, Wilton, CT.,06897-3902 or e-mail at: ndpool@juno.com.

PROTO-FILES - Right are three prototype bridge photos just to give some fodder for modeling ideas. Top is a photo by Pete Mihelich of a Wisconsin highway "rainbow" bridge over the Union Pacific Railroad (formerly C&NW). Middle is another rainbow bridge in Kentucky over the Norfolk Southern south of Cincinnati. Bottom is a combo deck and through girder bridge in Elkins, WV. Bottom two photos by Jeff Madden

PROTO-FILES



EXTRA BOARD

DEPT. OF CORRECTIONS

Take out your black pen and fix the width on the Chester Valley track plan in the December *Dispatch*. It should be 12', not 15'. Although it would be OK to use 15', the 12' measurement is more practical for a typical layout. Also, we finally have definite dates for the 2009 S Spree. The dates I listed in the December issue were guestimates, and unfortunately my source wasn't accurate. The dates are April 30-May 2.

SAN DIEGO SHOW

The members of the San Diego S Gauge Club in its first year presented "S" to San Diego at the Great Train Expo in Del Mar in late December, 2008. This is the first time that "S" has been represented here for years. The S-Mod modular layout featured a DCC mainline and an AC mainline all on S-Helper flextrack and Tom's Turnouts.

S IN NON-S PUBLICATIONS

- Dec. 2008 *CTT*: 2 all-gauge articles - one on a drive-in DVD movie theater and one on structures by Vic Roseman. Plus, one page on the AF 5007A set.
- Jan. 2009 *CTT*: A Roger Carp article on Jack Colls hi-rail S layout.
- Feb. 2009 *CTT*: One page on the AF K775 Baggage Loading Platform.
- Mar. 2009 *CTT*: 4-page article by Joe Deger about the Frontiersman trains.

2008 SPREE CARS AVAILABLE

For those who didn't attend the 2008 S Spree, The Miami Valley S Gaugers are announcing they still have some of the SHS CH&D special run boxcars available. They were pictured on the cover of last February's issue. Check the website for details:
www.trainweb.org/mvsg/.

FOR SALE

Member Neil Redding is selling

several S scale items. Most equipment has scale wheels and 802 couplers and are packed in original boxes. He has 3 SHS TOFC flats - NYC, PRR and NH with matching trailers - \$30 each. He has a set of NYC SHS F3s (A&B) plus several passenger cars - AM NASG 10-1 Pullman, SHS (AM) RPO, combine, coach and obs - plus an extra coach - all \$325. Another set includes NYC Empire State Express AM J3a streamlined Hudson plus four car set of AM Budd cars plus 1 extra coach - all \$325.00. S&H - flats \$10 each, sets \$30. Check or postal money order. You can find his address in the directory or email: nar@twcny.rr.com.

S-TIPS

Laying SHS Flex Track

The Central Ohio S Gaugers just re-did the inside loop of our 4-loop club layout with S Helper Flex Track. We did clean off the weathering on the ends of the flex track using a wire brush on a Dremel. Then we took a toothpick and put a small amount of Oatey #53019 "Instant Solder" (containing solder and flux) on the rail ends and some in the rail joiners. We slipped the rail joiners on the flex track (notice there are no "nibs" at the ends so joiners slide on easily). We did this with three sections which is enough to do our 42" radius on one corner. Then using a resistance soldering gun (the way to solder here), we soldered the three sections together.

The resistance unit is pinpoint, so not much chance of melting the near-by ties. Next, using 3 people, we curved this long section to lay on top of the roadbed and set it back to put down Liquid Nails using a flat spreader to evenly coat the roadbed. We purposely marked the end of this long section about an inch back from each end and DID NOT coat this area with adhesive. This was so we could attach the next section and then easily put ties under the rail joiner areas to maintain uniformity of the spacing.

Interestingly, when the Liquid Nails had a slight haze on it, the three fellows picked up the long sections and laid it down on the roadbed. The flex track sort of held its curve in memory, so it was easy to put back down. I was impressed with this "behavior" of the SHS flex track. Next we placed some bricks close together on the track and let it harden overnight. Then we cleaned off a few spots on the weathered rail with a small wire brush to solder the feeder drops. Again, we used the Oatey Instant Solder

For the extra ties that will go under the rail joiner "joints" you'll need to remove the plastic spike heads and even some of the roadbed beneath to get the extra ties to slide under easily.

- Alan Evans, COSG

Crossbuck Sizes.

Occasionally I see a report that crossbucks from another scale, or toy train crossbucks might be usable for S scale. Here's some data to help you decide if they are "good enough". According to Mainline Modeler, April 2002, the Union Pacific standard crossbuck design is as follows: Center of crossbucks - 10' 6" from ground. Crossbuck blades - 9" wide and 5' long. Post - 4" OD.

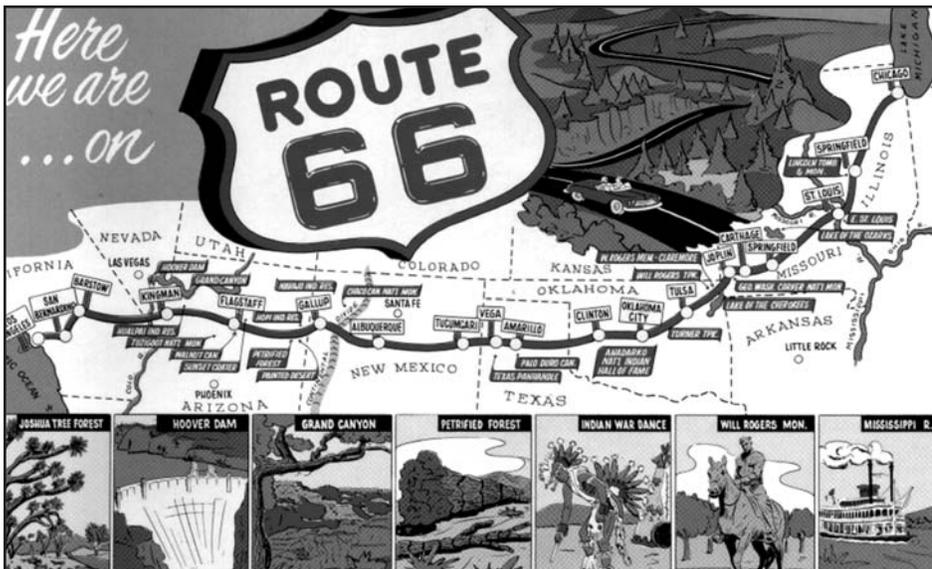
- Ted Larson

Toggle Turnout Throws

Have you thought about using a toggle switch (mini toggles) for a switchstand? That would allow you to power the frog as well as throw the points and wouldn't look any worse than an oversize O scale ground throw.

- Dave Branum





S Calendar

April 30-May 2, 2009: Spring S Spree, Strongsville, OH, Holiday Inn Select, sponsored by the Cuyahoga Valley S Gaugers. Gene Rominski, 440-234-5754, erominski7817@wowway.com www.cvsga.com.

May 1-3, 2009: Flyer Fest West, Sacramento, CA, sponsored by the Sacramento Valley American Flyer Club. Don Rosa, FFW2009@comcast.net or 916-682-6961

July 5-11, 2009: NMRA National Convention, Hartford, CT - www.hn2009.org.

Aug. 4-8, 2009: NASG Annual Convention, St. Louis Marriott St. Louis Airport as selected by the BOT. www.nasg2009.com. Jim Bresnahan at 573-372-3735.

Oct. 10, 2009: Annual Indianapolis S Show, hosted by the Hoosier S Gaugers, at the Carmel, IN Lions Clubhouse (just north of Indianapolis). Oct. 9 - Friday evening early bird get-together at Shapiro's Restaurant in Carmel. www.hossier-s.org.

Nov. 6-8, 2009: Fall S Fest, Clock Tower Resort, Rockford, IL, sponsored by the State-line S Gaugers. www.state-lines-gaugers.org.

April 30-May 1, 2010: Spring S Spree presented by the SE Michigan S Gaugers, Ford Community & Performing Arts Center, Dearborn, MI. Details later. This is the first in Michigan.

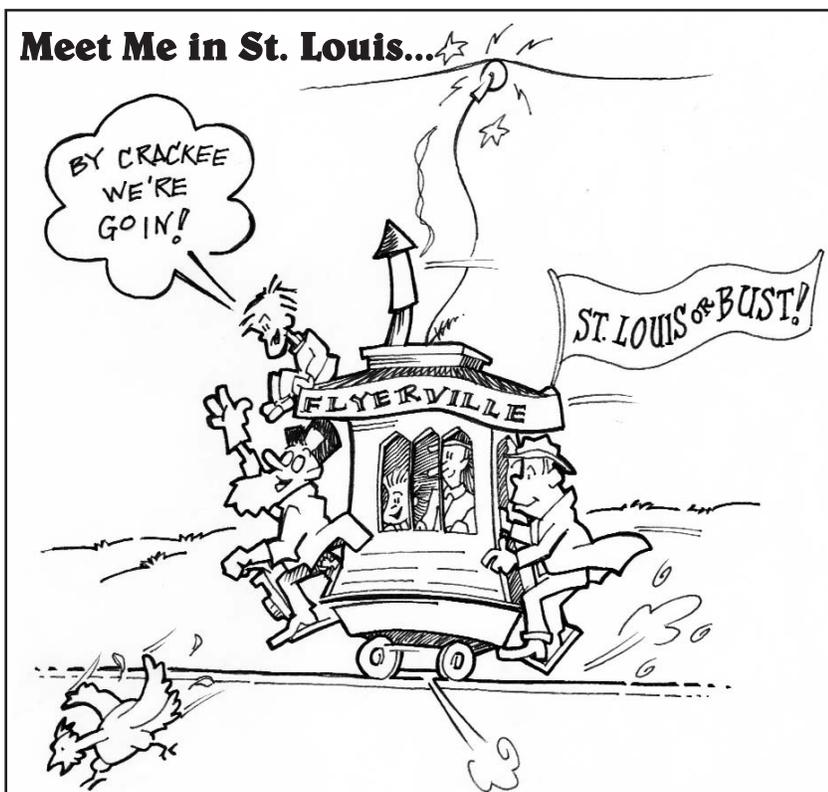
July 21-25, 2010: NASG Annual Convention, Duluth, MN. NASG 50th Anniversary. Details TBA.

Bids open for NASG Annual Conventions 2011 and beyond. contact Lee Johnson, 2472 Lariat Ln., Walnut Creek, CA 94596-6635. 925-943-1590 - email: leemax@jps.net.

Asleep At The Switch



Have a humorous train idea for Asleep At The Switch? Email it to aats@tchuck.com



Railmail cont. from page 5

how that was enforced, but nobody wants to stick the convention hosts with a loss.

- Bob Werre

And More on Lodging: Most often, the issue is not that no one wants lower costs, and since just about everyone wants convenience, it is no usually do-able. Cheap motels do not have the display area or clinic room space needed. I have attended a couple of narrow gauge conventions where the dealer room was in the convention center, the clinics in one hotel, and the contest rooms in another hotel – and none had decent parking. It was a true nightmare where we did not get to many clinics and never got to the contest room when it was open. Cheap motels, like Motel 6, will still cost \$50-\$90 a night depending upon location. A few places around the country have hotels with enough room for the S convention at good rates, but there are not many. The Indianapolis O meet has a good place, but who wants to go to Indy every year? So, folks complain about the hotel costs, or they complain about the inconvenience of not having all components of the convention under one roof. Ya cannot win!

- Bill Wade

A Little S Scale History: The reason for the National Association of S Gaugers is that way back in the early days that is what we were – a varied group of aspiring model railroaders who either collected American flyer and then chopped or mod-

ified all kinds of stuff in an attempt to run S scale trains. So, how many times do we have to attempt to get on the scale side of things? (Excuse the “WE”, meaning we S scalers). Our major push for S scale today is through our NASG, not that we scale modelers haven’t tried to change thing to our favor over the years. We published scale oriented magazines, we still have many brass S scale dealers, we persuaded our other manufacturers to insert scale wheel sets in their AF compatible models, and so on. Heck we even have an S scale NMRA SIG (Special Interest Group) that still needs some help. How many of you S scalers belong to that group? How many S scalers belong to the NASG, the NMRA?

What we have to face is, in fact, that we S scale modelers are on the bottom of the pile. Our manufacturers have to make money to stay in business, and they ain’t getting as much money from the scale group as they are from the AF R-T-R group. So who is willing to go for a name change? Who is willing to quit the great (\$20) a year dues for the NASG and start up a new organization named the “NASS (National Association of S Scalers)? Folks, fifty years later (from the beginning of the NASG), we in S gauge are still a varied bunch of model railroaders trying to achieve scale operations. Frankly, I feel we are doing just fine the way we are. Look at the magnificent progress we’ve made in the last twenty years. Who would have believed it when American Flyer went belly up back in the 1960s of the progress we

made in S. Many, many thanks for this to Frank Titman, Wally Collins, the S Gauge Herald, and all of us in S gauge who kept 3/16ths modeling alive until we could rebound with American Models and S Helper Service among many manufacturers. As I said before, get off the what if “merry-go-round. Enjoy your model railroading hobby, what ever aspect of it you are in. Run the trains.

- Bill Fraley

Scalers Should be Content: I have never really understood the complaints about Flyer. I do not collect Flyer and do not purchase it. If Flyonel would crank out that Mike in scale, my appreciation for the company would increase in geometric proportions. That apparently is not likely to occur. Those who purchase S rolling stock with big flanges and huge couplers outnumber us scalers probably 10 to 1. It is their money that funds what we have now. Let’s be happy we have a journal and the NASG to which I need to renew my membership at the moment. I’m happy to see the hi-railers and am extremely thankful for their support. It does appear that their knowledge of prototypes has risen over the years, something to which the Flyonel Mike attests. Better that than a reissue of a Reading Atlantic with its out-of-scale low drivers and lack of boiler detail.

- Tom Baker

No Separate Group: The original goal of the NASG was to unite S gaugers, not divide them. Claud Wade (one of the founders) had Flyer, so did Bernie Thomas, Gene Fletcher, Tom Coughlan and Frank Titman (to name a few). I can list countless modelers who used Flyer as a path to hi-rail and then to scale. Don Riley was a “wizard” with at this). The organization as it is serves as a stepping stone to bring toy train operators more into the realm of scale modeling.

- Raleigh in chilly Maine

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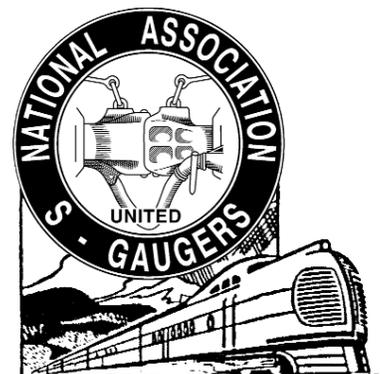
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Jeff's Jct. cont. from page 5

there should be many in this economy.

4. Be a Seller: If you're a manufacturer or a dealer you can write off expenses. If you're just a P.O.M. (plain old member) you can still have a sale table or two. I don't know of any model railroader who doesn't have items he can dig up to sell. At the very least you can pay for what you buy, or if you buy more than you sell, at least you won't feel as guilty.

5. Bring a Layout: If you have your own portable layout or belong to an S club with one, many sponsoring clubs will pay some expenses. If you're not sure about this, ask.

6. Food: Save on food. Stay at a hotel with a free breakfast, especially if it's the convention hotel. If you're in a room with a fridge, get lunch stuff and make your own P&J or baloney sandwiches. Plan your bigger meal expense for dinner and the banquet and scrimp on breakfast and lunch. Of course, you can skip the banquet if you must. The real "cheap" way is to bring an RV and cook all your own meals or just eat at fast food places with a 99 cent menu – but that ain't no fun.

So let's recall our saying – **"Where there's a will there's a way."** Throw away all those excuses and have some will! The economy be damned! See you in St. Louis or Cleveland or Santa Clara, California or South Amboy, NJ or Indianapolis, or Rockford, Illinois this year. I'll be at many of them.



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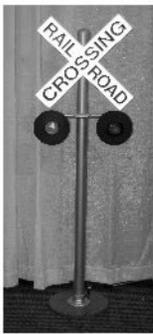
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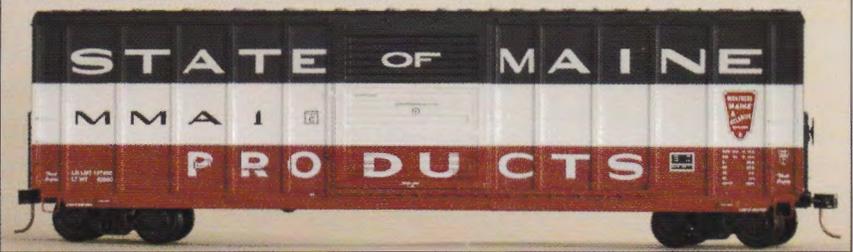
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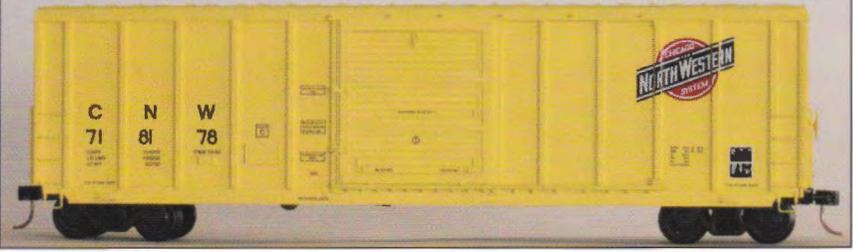
MMA - State of Maine

SSA2101.1 #1
Only car painted is this scheme.



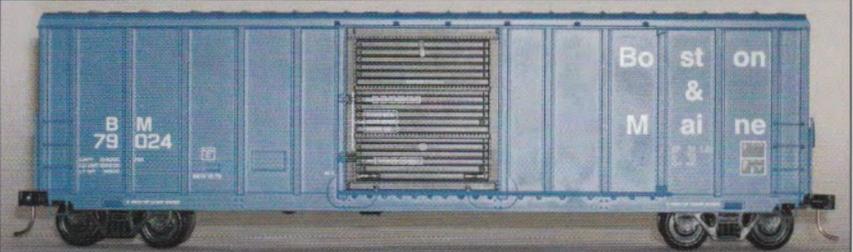
Chicago & North Western

SSA2102.1 #18178
 SSA2102.2 #18194
 SSA2102.3 #18222
 SSA2102.4 #18251



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SSA2103.1 #9024
 SSA2103.2 #9054
 SSA2103.3 #9066
 SSA2103.4 #9079



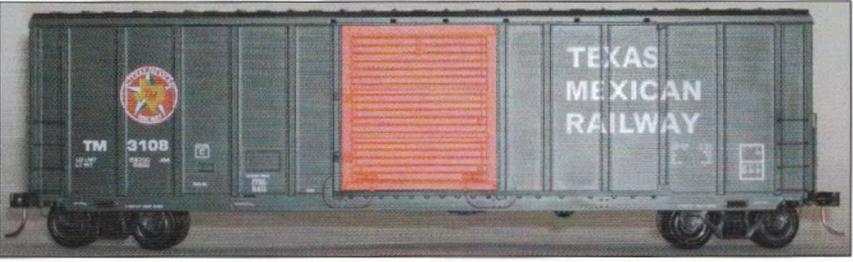
Apalachicola Northern

SSA2104.1 #5582
 SSA2104.2 #5597
 SSA2104.3 #5603
 SSA2104.4 #5614



Texas Mexican

SSA2105.1 #3108
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